

Landmark / Van Dorn Area Planning

City of Alexandria, Virginia

Public Meeting # 3

Planning and Design Workshop

June 15, 2005



Wallace Roberts & Todd, LLC

1

Landmark / Van Dorn Area Planning

Workshop Agenda

- **Consultant Team Presentations**
 - Study Purpose and Process
 - Transportation
 - Market Overview
 - Land Use and Urban Form
- **Workshop Discussions (at breakout group stations)**
 - Transportation
 - Land Use and Urban Form
- **Summary and Planning Commission Direction**

2

Study Purpose

Purpose of Planning Effort

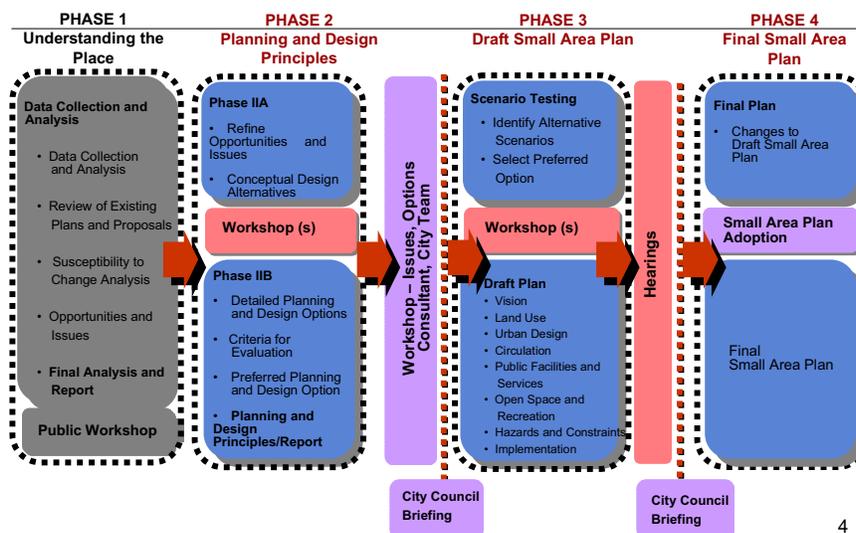
- Develop a long-range plan for the Landmark-Van Dorn Small Area, including planning and design principles to guide public and private investment
- Address the role of the Landmark Mall site in the overall area and its relationships to surrounding properties (land use, urban design, access and circulation, etc.)

Purpose of Public Meeting #3: Begin developing the detailed Landmark / Van Dorn Small Area Plan

- How can the Planning and Design Principles be applied to the planning area, focusing on the key issues of concern?
- How can the different “pieces” fit together to create a coherent community and sense of place?

3

Planning Process



4

Planning Process

- **Public Meeting #2**

- **Confirmed / Refined Planning and Design Principles**

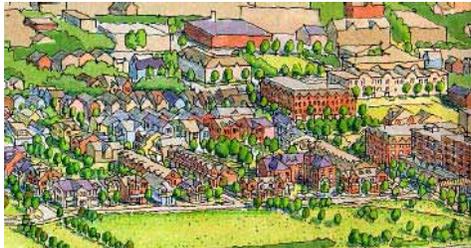
- Community of Neighborhoods
- Multi-Modal Transportation System
- An Interconnected Open Space System
- An Attractive Gateway to Alexandria

- **Identified Key Issues of Concern**

- Existing and future traffic conditions
- Improving access and frequency of transit
- Increasing the supply of affordable housing
- Retaining existing business
- Changing from suburban to urban form
- Developing more community oriented facilities (e.g., usable open spaces and civic uses)

5

Planning and Design Principle 1. A Community of Neighborhoods



- Establish mixed-use activity centers as focal points of community activity, ranging in size from smaller centers convenient to neighborhoods to a large “Town Center” for the West End.
- Provide a range of housing types and price levels, including affordable housing as a priority.
- Retain and support local businesses.
- Improve safety and security for residents as the Landmark / Van Dorn area is redeveloped.

6

Planning and Design Principle 2. **A Multi-Modal Transportation System**



- Address vehicular mobility through targeted capacity improvements and by developing a more interconnected street system.
- Develop a safe, pedestrian-friendly environment with walking connections from neighborhoods to activity centers.
- Develop a system of on-street lanes and off-street paths for bicyclists.
- Provide more frequent, reliable transit service with connections to and between activity centers and the Van Dorn Metro Station.
- Encourage creative parking solutions while ensuring that parking lot location and appearance supports good urban form.

7

Planning and Design Principle 3. **An Interconnected Open Space System**



- Develop a network of public parks and open spaces of varying sizes and functions and connected by linear corridors.
- Provide public outdoor spaces within activity centers.

8

Planning and Design Principle 4. An Attractive Gateway to Alexandria



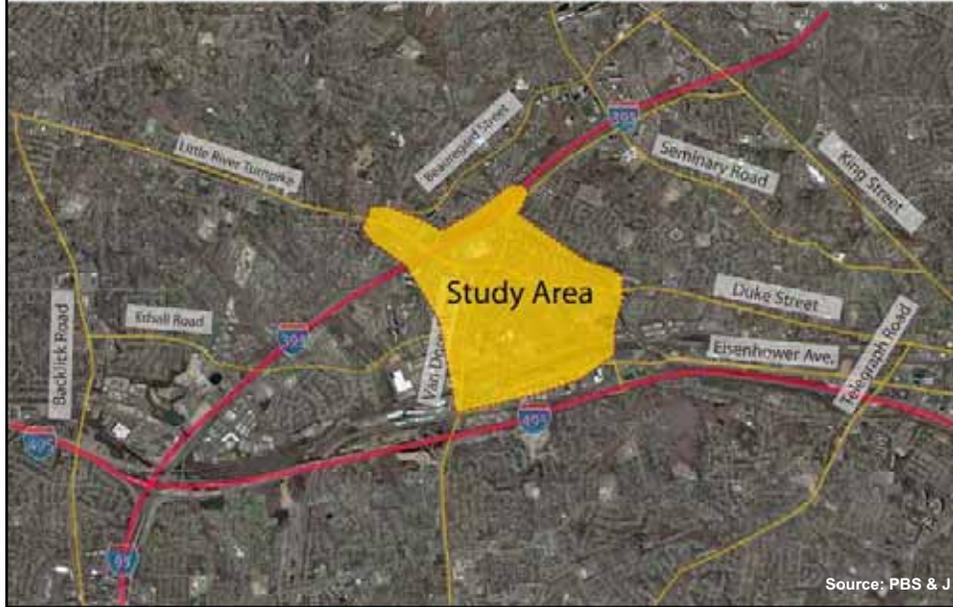
- Design gateway improvements at entrances to the City to build a distinctive visual image and identity for the West End.
- Improve the visual character of major roadway corridors such as Van Dorn Street through streetscape improvements.
- Promote good urban form by establishing guidelines for site and building design.

9

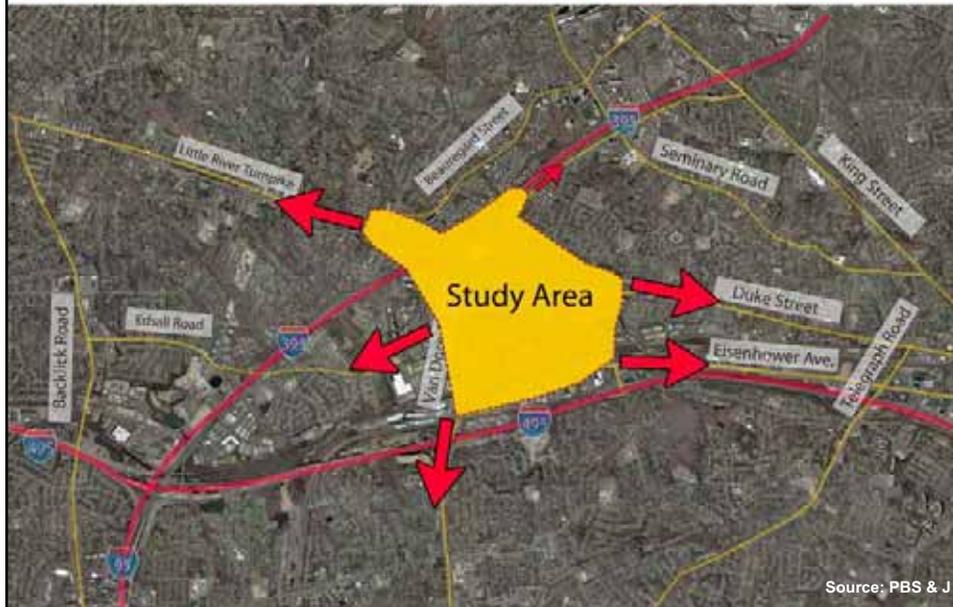
Regional Transportation Network Roadway Network



Regional Transportation Network Study Area



Regional Transportation Network Limited Connections to Surrounding Areas



Existing Conditions Analysis
Year 2005 Weekday Segment Levels of Service



Source: PBS & J

Future traffic volumes based on regional transportation and land use model assumptions.

Level of service estimates are planning-level estimates of peak-hour performance based on number of lanes and average daily traffic, showing the expected performance of roadways of this type in typical urban environments. 13

Existing Conditions Analysis
Year 2005 Saturday Segment Levels of Service

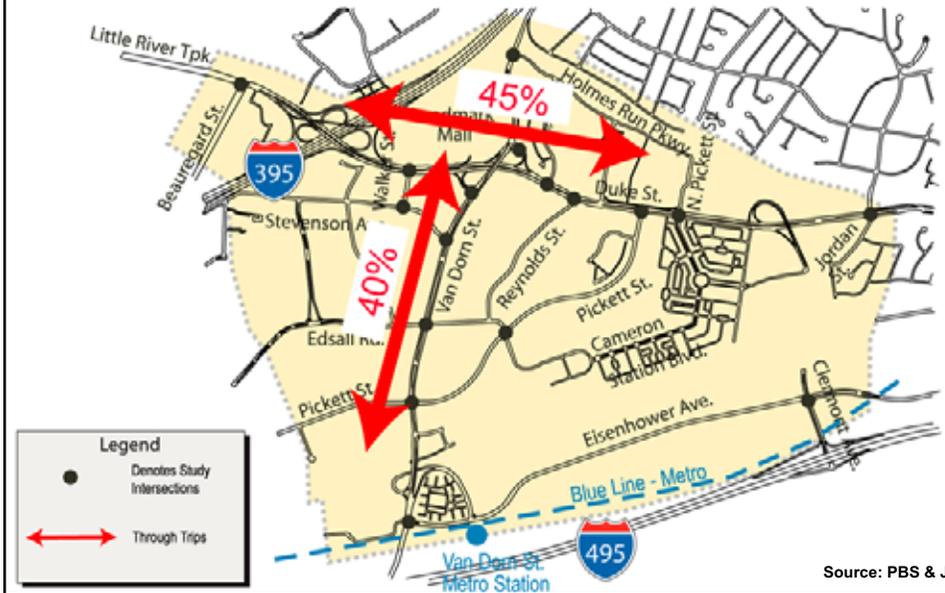


Source: PBS & J

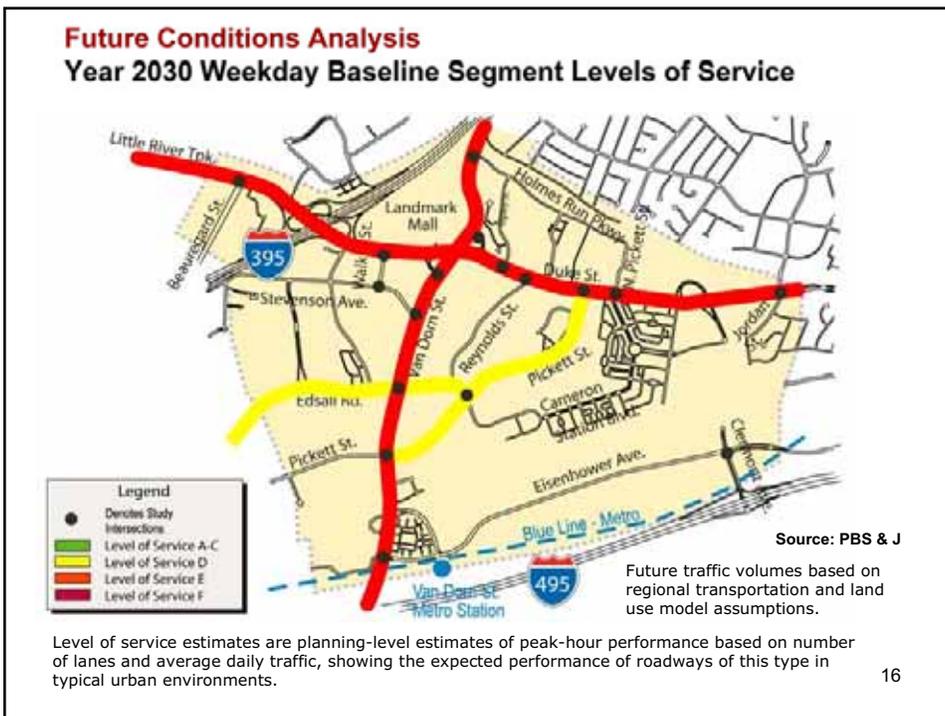
Future traffic volumes based on regional transportation and land use model assumptions.

Level of service estimates are planning-level estimates of peak-hour performance based on number of lanes and average daily traffic, showing the expected performance of roadways of this type in typical urban environments. 14

Existing Conditions Analysis Through Trip Percentages



Future Conditions Analysis Year 2030 Weekday Baseline Segment Levels of Service

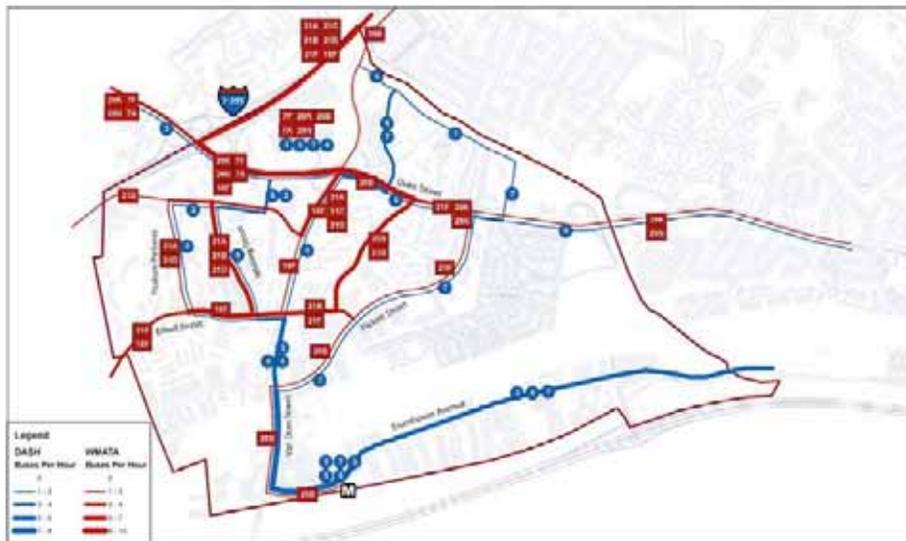


Future Conditions Analysis Year 2030 Saturday Baseline Segment Levels of Service



Level of service estimates are planning-level estimates of peak-hour performance based on number of lanes and average daily traffic, showing the expected performance of roadways of this type in typical urban environments.

Existing Peak Hour Transit Service



Landmark Sub Area Study- Existing Peak Hour Transit Service

Existing Off Peak Transit Service



Landmark Sub Area Study- Existing Off Peak Transit Service

Source: BMI.SG

Evaluation of Existing Transit Service

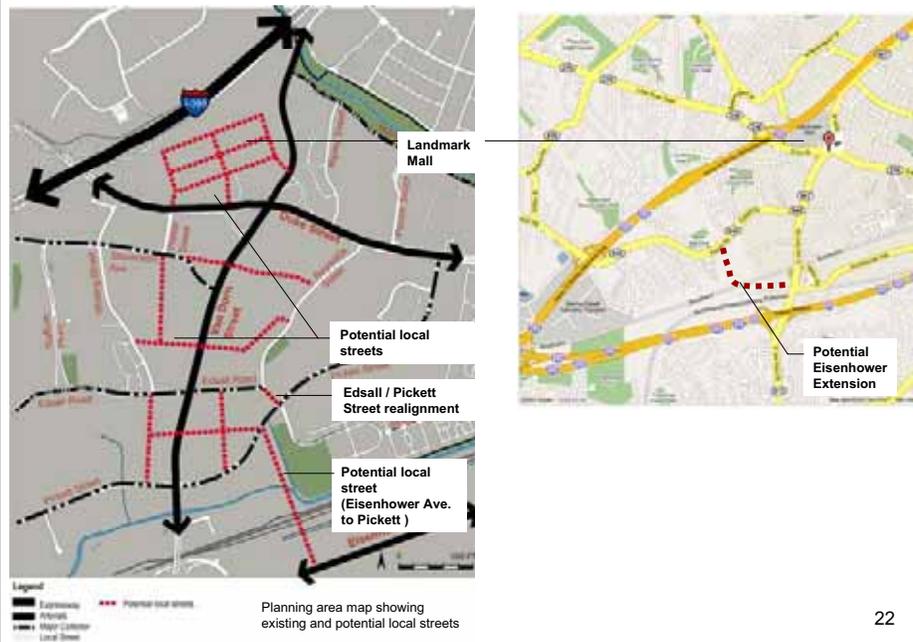
- No coherent theme
- Uncoordinated scheduling
- Excessive time and maneuvering required to enter/leave Landmark Mall
- Transit stop at Landmark “hidden” (not visible or prominent)

Transportation Ideas for Draft Small Area Plan



21

Improving Mobility: Potential Street Connections for Vehicles, Pedestrians and Bicycles



22

Improving Mobility: Transit Service

Transit Service must be:

- Frequent
- Reliable
- Clearly Identifiable



23

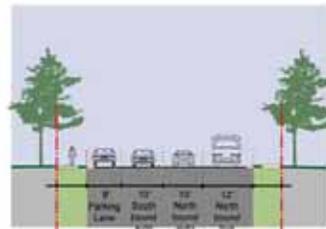
Source: BMI.SG

Improving Mobility: Potential Transit Circulator Routes

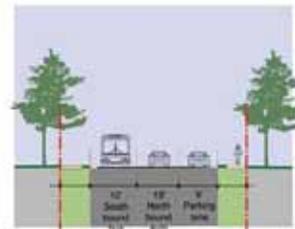


Conceptual routes for new local transit shuttle

Source: BMI.SG



Local Street

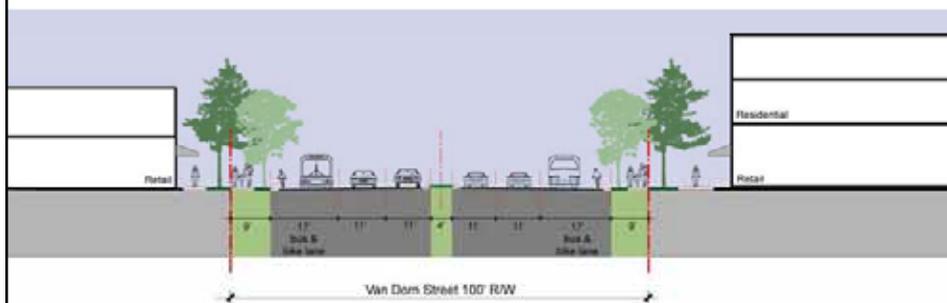


Local Street

Transit emphasis local street examples²⁴

Balancing Regional and Local Traffic: Van Dorn Street

Option A: Van Dorn Street as Pedestrian Friendly Local Street

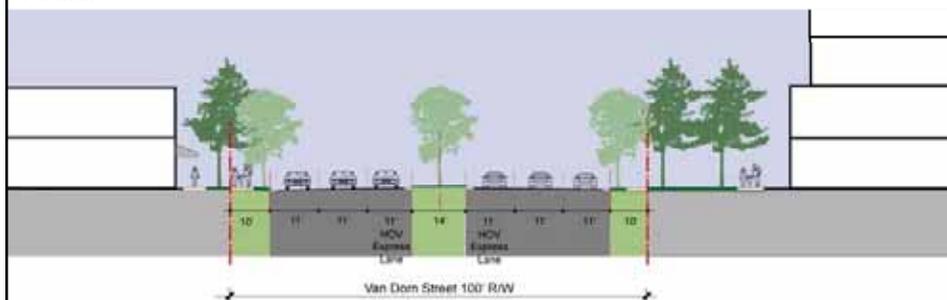


- Slow, pedestrian friendly traffic
- Two northbound and southbound local traffic lanes for vehicular traffic
- One northbound and southbound transit service and bike lane

25

Balancing Regional and Local Traffic: Van Dorn Street

Option B: Van Dorn Street as Regional Arterial with Pedestrian Friendly Features

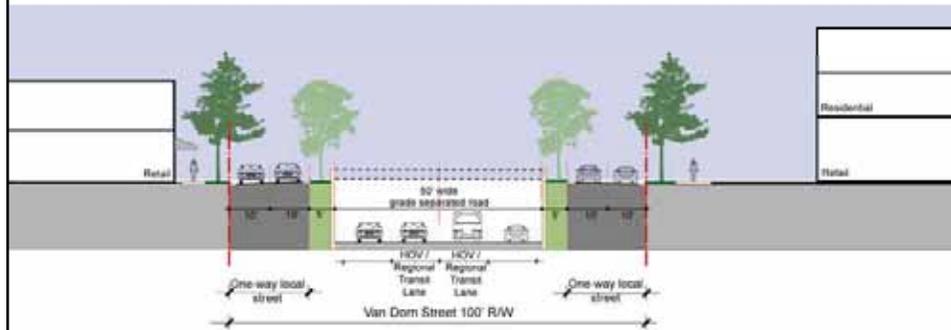


- HOV express lanes for regional traffic
- One northbound and southbound local traffic lane
- Transit through other local roads

26

Balancing Regional and Local Traffic: Van Dorn Street

Option C: Van Dorn Street as Regional Arterial and Local Street



- Grade separation of regional and local traffic
- Local transit service on local Van Dorn or other local streets
- Regional transit service through grade separated road

27

Van Dorn Street: Current Condition



28

Van Dorn Street: As Pedestrian Friendly Street



Landmark Mall / Duke Street: Current Pedestrian Access



Landmark Mall / Duke Street – Improved Pedestrian Access



Market Feasibility Analysis for Selected Land Uses

Study Background & Objectives

- Market Inputs into Planning Process
- Demand Conditions over 10-Year Planning Horizon
- Preliminary Development Feasibility and Implementation Strategies
- Key Issues:
 - What are the drivers of future development?
 - Future of existing uses – industrial, affordable housing stock, etc.
 - Viability of more urban development
 - The impact of catalytic development

Location Analysis

Overall, available sites within the Landmark-Van Dorn area represent strong redevelopment opportunities.

- Infill Location in Rapidly Growing Region
- Strong Demographics in Broader Market Area
- High Visibility and Accessibility
- Large Underutilized Sites
- Challenges of Current Built Environment Can Be Overcome

Factor	Grade
Regional Location	A-
Demographics & Economics	B
Access & Visibility	B+
Development Opportunities	A-
Character	C+
Overall	B+/A-

Summary of Market Findings

- Redevelopment *and* Repositioning
- Residential Development is the Driver
 - 1,500 to 2,200 net new units
- The Landmark Mall as a Redevelopment Anchor
- Creation of an Urban Retail Activity Center
 - 500,000 to 600,000 SF
- Neighborhood Retail Opportunities



Summary of Market Findings



- Office Development as a Complementary Use
- The GSA Office Niche
 - Is this a likely location?
 - Is this type of use compatible?
- Threats to Existing Uses
 - Affordable Housing
 - Flex/Industrial
- Mixed Use vs. Integrated Multiple Use
 - Mixed Use: Targeted opportunities where premiums justify cost
 - Integrated Multiple Use: Maintaining the urban character with less risk

Land Use

Existing Land Use



Parcels Most Susceptible to Land Use Change



37

Land Use

Potential Activity Centers



- Activity Centers should have sense of place, character and pedestrian friendly environment.
- Three potential locations within the area (Sites A, D and H)

Other Development Opportunities



- Other development opportunity sites (Sites B, C, E, F, G, I, J and K)

38

Potential Land Uses

Potential (Market Driven) Uses

Source: Robert Charles Lesser & Co., LLC

- Mixed use activity centers (mid-box/restaurant anchors, smaller/specialty retail, residential, etc.)
- Multi-family residential (for-sale, rental)
- Neighborhood-serving retail (restaurants, specialty shops, convenience, etc.)
- Smaller office uses

Potential Market Driven Replacements

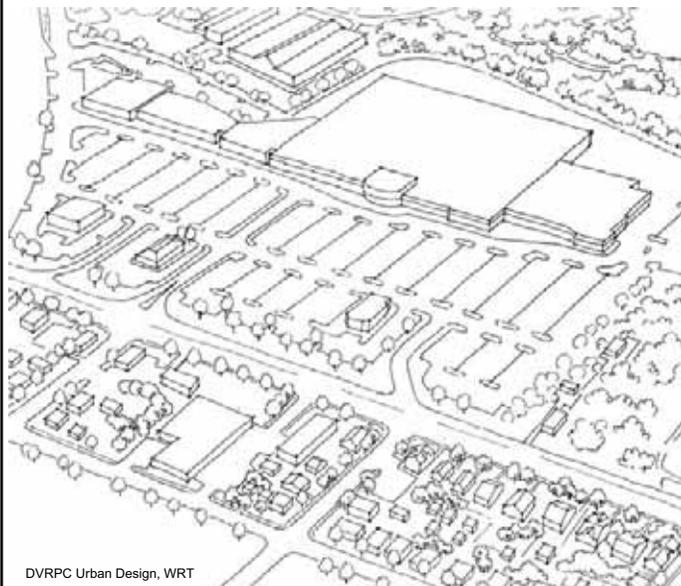
- Flex/industrial uses
- Some rental properties

Community Oriented Uses

- Increasing supply of rental and/or affordable housing
- Community facilities
- Active usable open space

39

Urban Form Existing Density



DVRPC Urban Design, WRT



40

Urban Form Transitional Density



Urban Form Urban Density



Urban Form Activity Centers



Clarendon Commons, Source: Antunovich Associates



43

Principle: A Multi-Modal Transportation System

Questions for Discussion

- Vehicular Traffic

Considering the options for Van Dorn Street, what is the best approach to balancing regional and local traffic?

- Connectivity

Where are the opportunities for connections to improve mobility for vehicles, pedestrians, bicyclists, and transit?

What specific kinds of pedestrian improvements should be provided?

- Improving Transit Service

What specific transit services are needed that we do not have now? Where should these services be provided?

44

Principle: A Community of Neighborhoods

Principle: An Interconnected Open Space System

Principle: An Attractive Gateway to Alexandria

Questions for Discussion

- Land Use

What land uses and community-serving functions should be provided in the area and where?

- What uses should be provided in Activity Centers? Where should these centers be located?
- What uses should be provided on other development opportunity sites?
- Where are desired locations for public open spaces?

- Urban Form

What is your vision of good urban form for the area in terms of building height, mass, and scale; density; the character of public streetscapes and spaces; etc.?

How can the different elements expressed in the Planning and Design Principles be integrated to promote a coherent community and sense of place?