

Chapter 9: Neighborhoods Specific

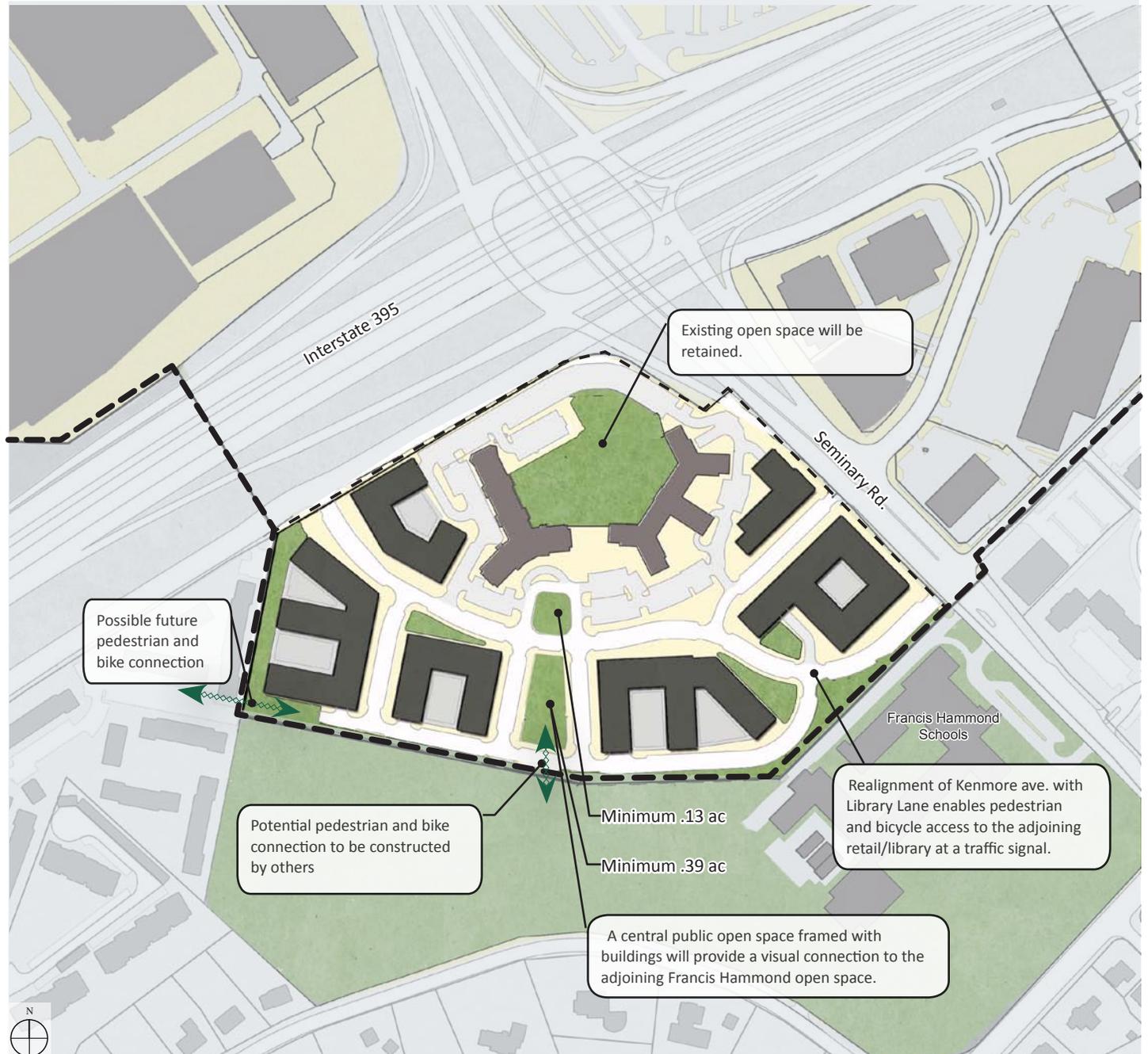
This chapter contains specific standards for each neighborhood in the CDD #21 and #22 area. These provisions will ensure the plan goals for a mix of uses, interconnected street network, and variety of high-quality open space are realized with plan implementation.



a) Seminary Overlook

Diagram 9.a.1 Seminary Overlook Illustrative Plan

-  BSAP Boundary
-  Seminary Overlook Boundary
-  Existing Buildings to Remain
-  Proposed Buildings
-  Open Space
-  Parking Structure
-  Alley & Service Parking
-  Streets
-  Potential Pedestrian Connection



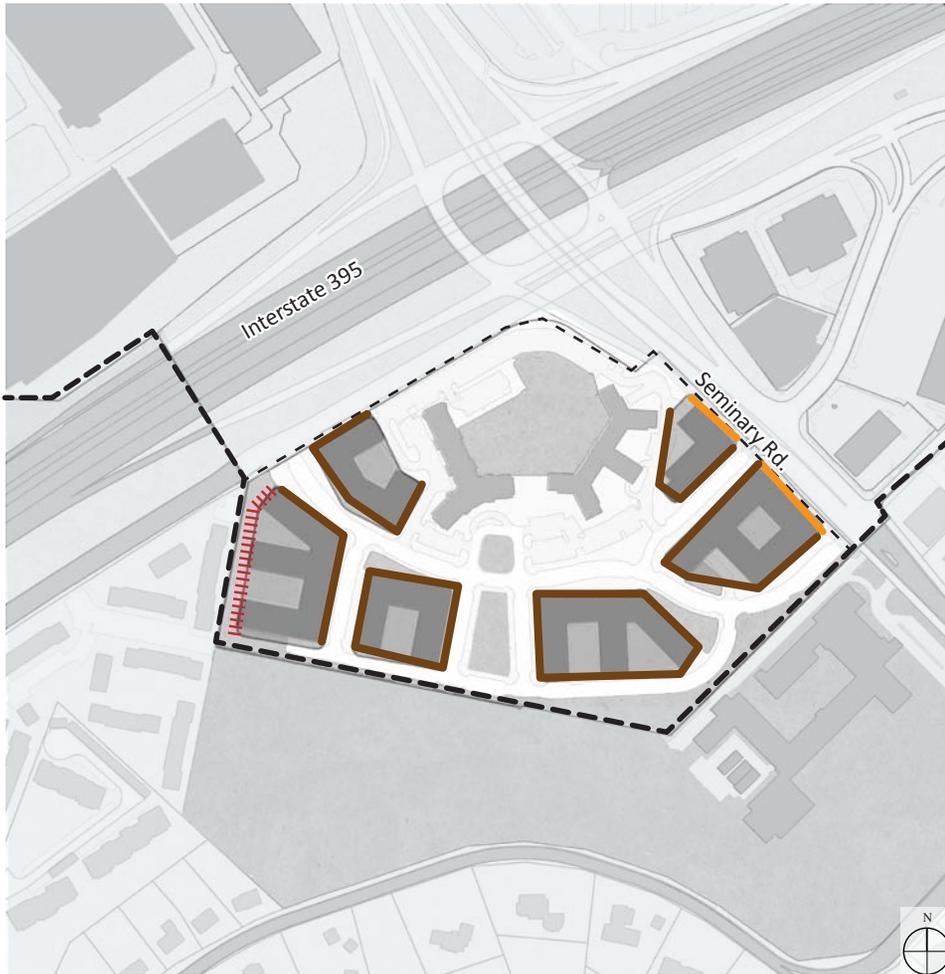


Diagram 9.a.2: Building Height Transitions and Setbacks

- ||||||| Required Building Transition (See Chapter 4, Section D)
- Frontage Setback Seminary Rd. (20' min. from edge of curb to building)
- Frontages shall have an average setback of 10 feet (from property line to building) for a minimum of 30% of the identified frontage for each building.

Notes:
All other setbacks shall be by building use as described in Chapter 4, Section C.

MARCH 5, 2013

ST - 62-34

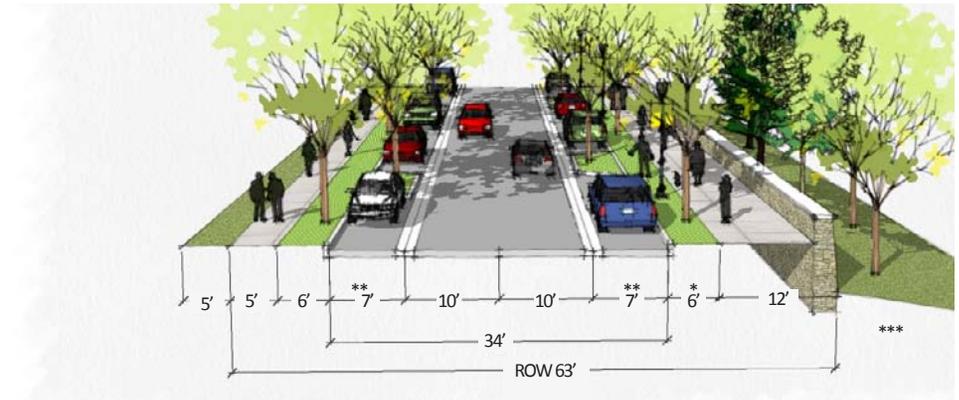
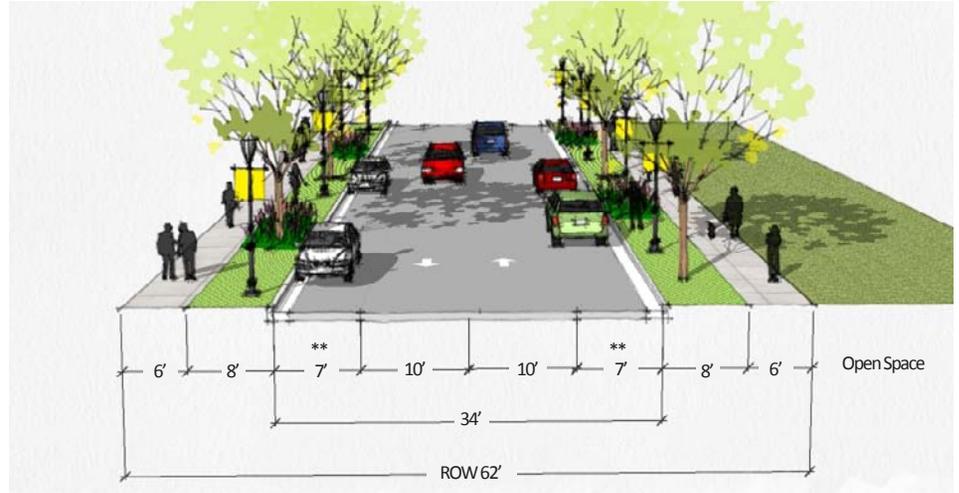


Diagram 9.b.3: Non-Framework Street Sections

- ST - 62 -34
- ST - 63 -34

Note:
R.O.W. based on existing condition
Sections may be finalized during the DSUP process
*Planting strip at urban locations may be tree wells
**On-street parking dedicated to Seminary Towers
*** Wall depicted for illustrative purposes only



b) Southern Towers

Diagram 9.b.1: Southern Towers Illustrative Plan

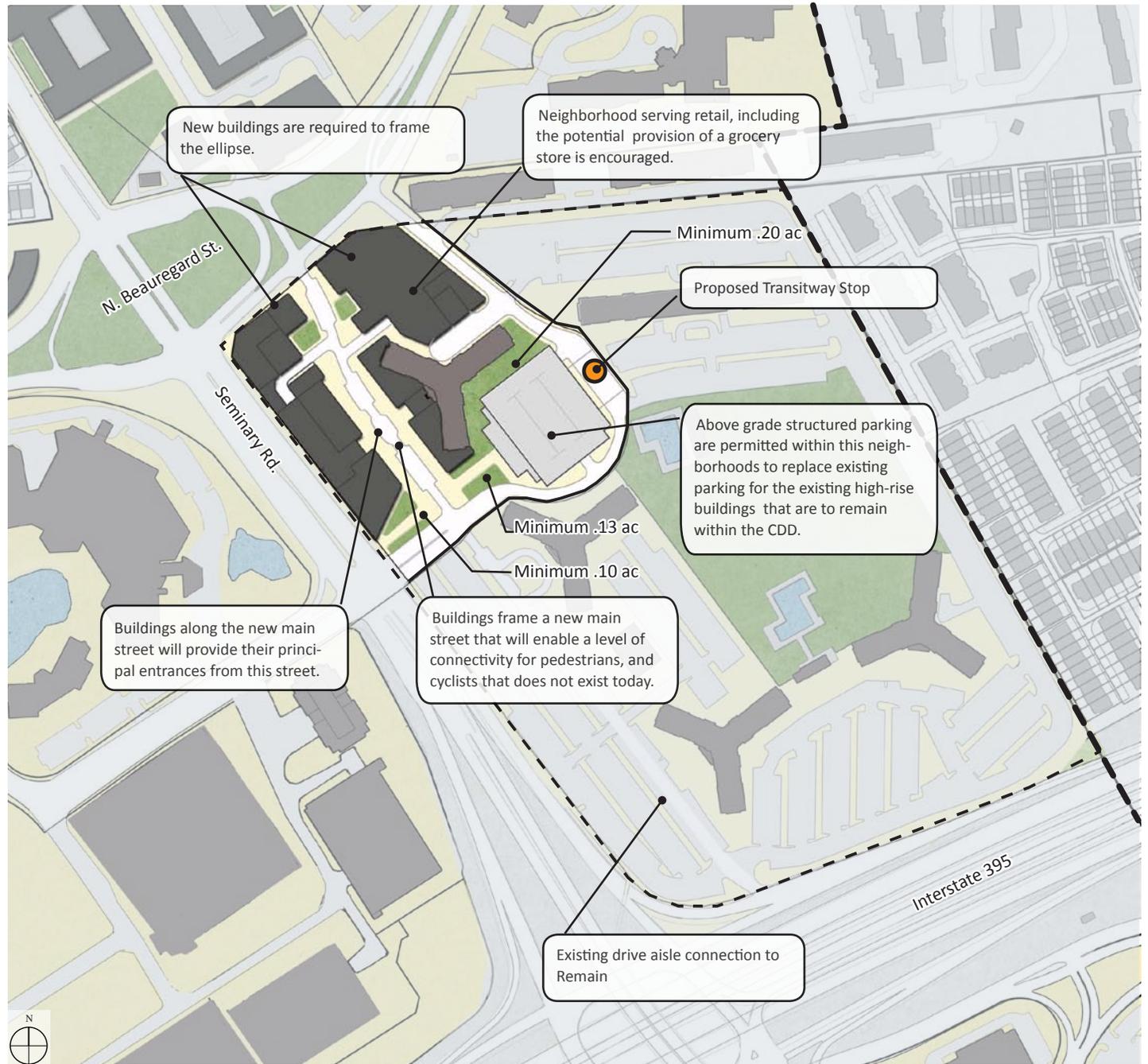
- BSAP Boundary
- Southern Towers Boundary
- Southern Towers CDD #21 Boundary
- Existing Buildings to Remain
- Proposed Buildings
- Open Space
- Parking Structure
- Streets
- Proposed Transitway Stop

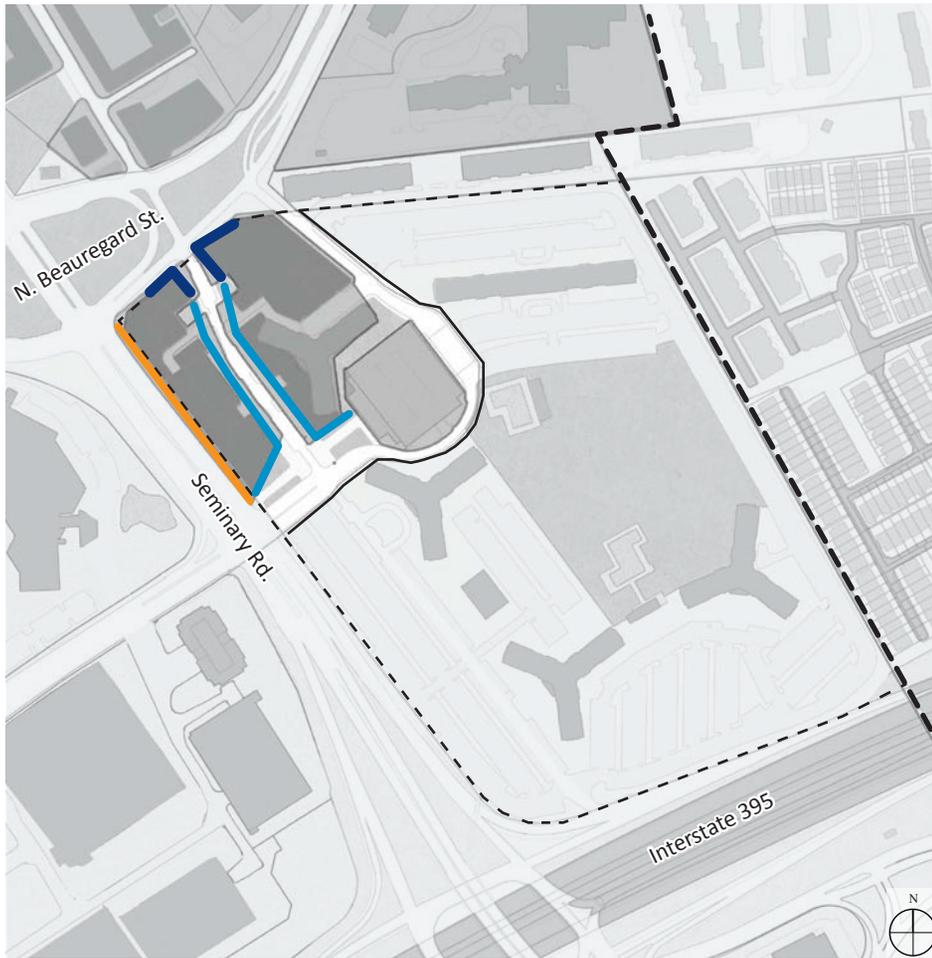


Notes:

Southern Towers has existing buildings, infrastructure, and amenities which will remain in operation during and after the new development. Additionally many of Southern Towers surface parking lots will remain. In some circumstances, existing conditions may preclude the application of some of these standards.

In locations where there are existing vehicular/parking entrances, structures and/or development, some requirements of these standards may not be feasible including, but not limited to: surface parking lots and block size/thoroughfares.





All street section within Southern Towers neighborhood are framework streets and shown within Chapter 8 - Street Standards and Guidelines

Diagram 9.b.2: Building Setbacks

- Frontage Setback Seminary Rd. (20 feet min. from edge of curb to building)
- Frontage Setback for Required Retail (16 feet - 25 feet from edge of curb to building)
- Frontage Setback for Optional Retail

Notes:

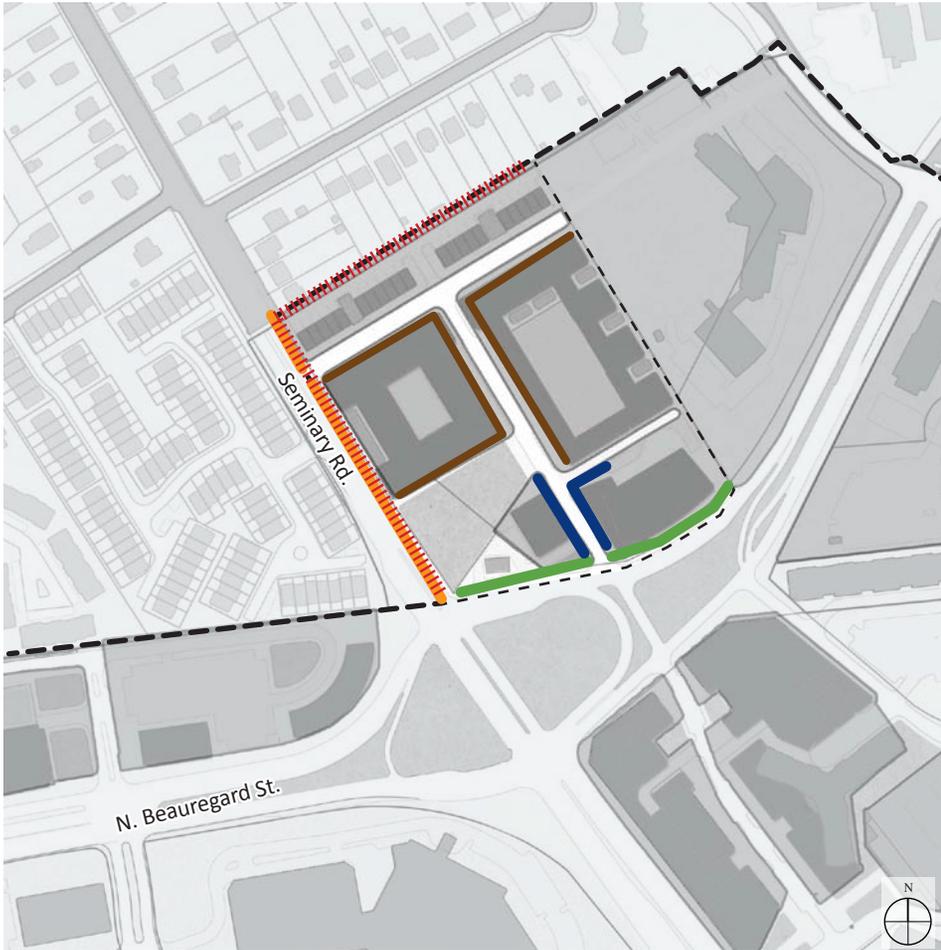
All other setbacks shall be by building use as described in Chapter 4, Section C.

c) Upland Park

Diagram 9.c.1: Uplands Park Illustrative Plan

-  BSAP Boundary
-  Upland Park Boundary
-  Existing Buildings to Remain
-  Proposed Buildings
-  Open Space
-  Parking Structure
-  Alley
-  Streets





All street section within Upland Park neighborhood are framework streets and shown within Chapter 8 - Street Standards and Guidelines

Diagram 9.c.2: Building Setbacks and Transitions

- Frontage Setback For Seminary Rd. (20' min. from edge of curb to building)
- Frontage Setback for N. Beauregard St. (10' min. from right-of-way to building)
- Frontage Setback for Retail (16 feet - 25 feet from edge of curb to building)
- Frontages shall have an average setback of 10 feet (from property line to building) for a minimum of 30% of the identified frontage for each building.
- ||||| Required Building Transition (See Chapter 4, Section D)

Notes:

All other setbacks shall be by building use as described in Chapter 4, Section C.

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d) Adams

Diagram 9.d.1 Adams Illustrative Plan

-  BSAP Boundary
-  Adams Boundary
-  Existing Buildings to Remain
-  Proposed Buildings
-  Open Space
-  Parking Structure
-  Alley & Surface Parking
-  Streets
-  Proposed Transitway Stop
-  Stop Potential Pedestrian Connection & Bicycle Connection
-  Mid-Block Passage



e) Town Center

Diagram 9.e.1: Town Center Illustrative Plan

-  BSAP Boundary
-  Town Center Boundary
-  Existing Buildings to Remain
-  Proposed Buildings
-  Open Space
-  Parking Structure
-  Streets
-  Proposed Transitway Stop
-  Mid-Block Passage



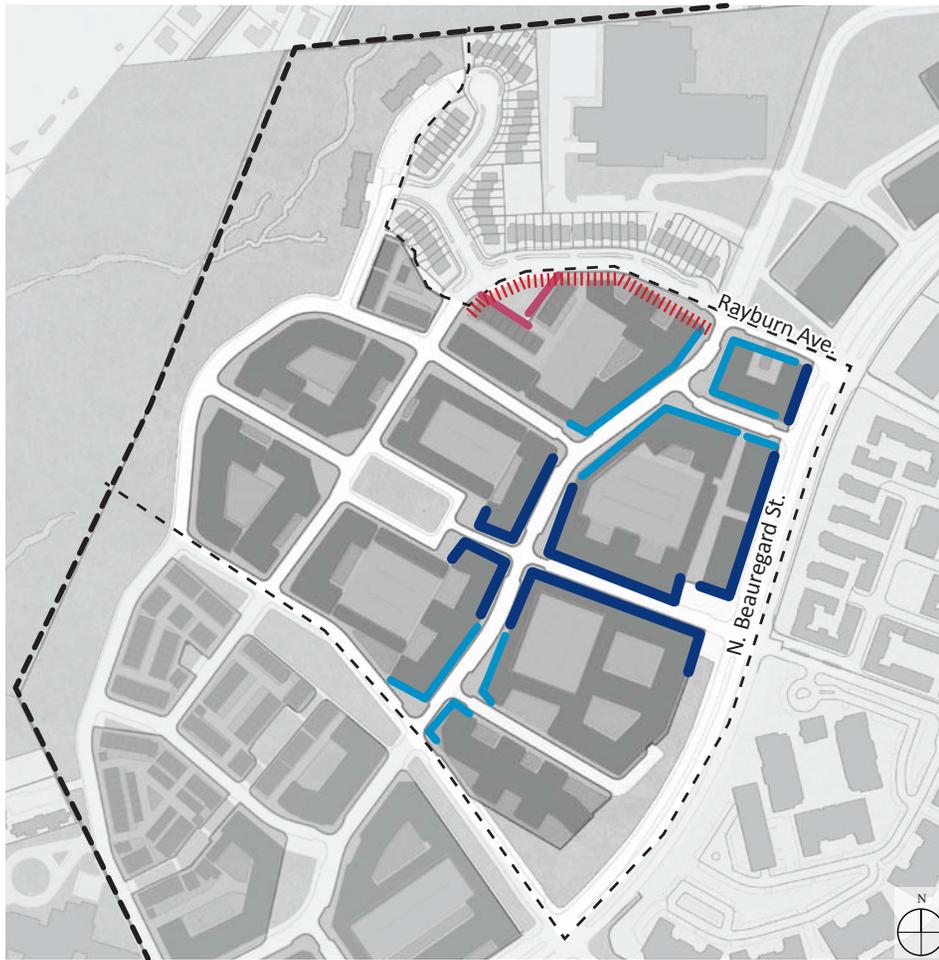


Diagram 9.e.2: Building Setbacks

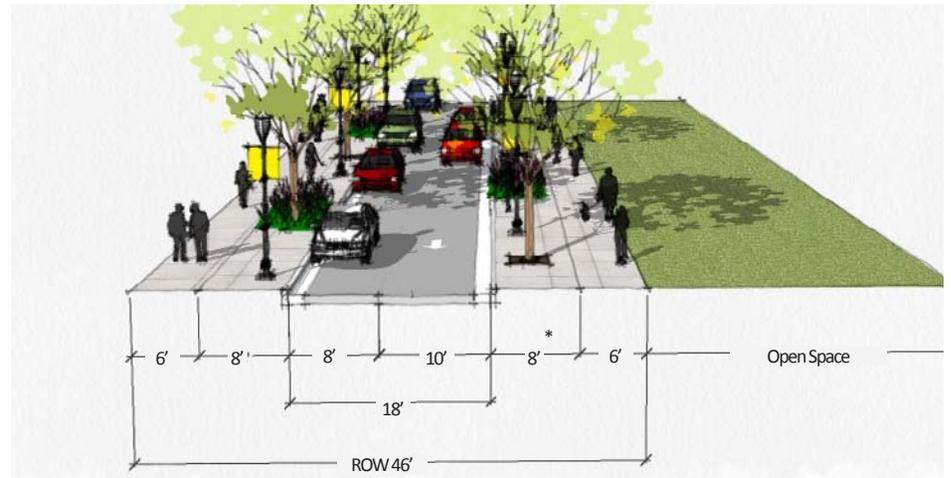
- Frontage Setback for Required Retail (16 feet - 25 feet from edge of curb to building)
- Frontage Setback for Optional Retail (16 feet - 25 feet from edge of curb to building)
- Frontage Setback frontage along Major Mid-Block Passages and Dedicated Open Space
- ||||| Required Building Transition (See Chapter 4, Section D)

Notes:

*Where townhouses are provided, they shall comply with setbacks requirements as set forth in Chapter 4, Section C.
 All other setbacks shall be by building use as described in Chapter 4, Section C.

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ST - 46 - 18



ST - 64 - 36

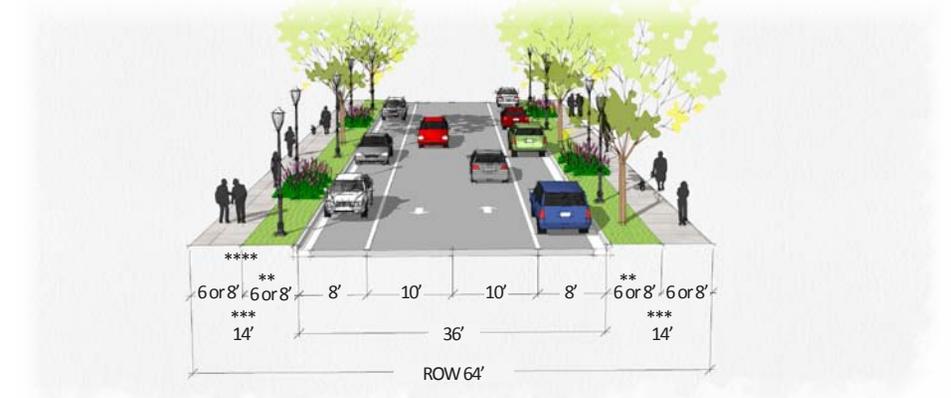


Diagram 9.e.3: Non-Framework Street Sections

- ST - 46 - 18
- ST - 64 - 36

Notes:

* The adjoining sidewalks will need to comply with all applicable City load rating requirements for fire truck as deemed necessary as part of the development review process.
 **Planting strip at urban locations may be tree wells
 *** Where 6 feet sidewalk is provided the landscape strip or the tree wells shall be increased in width to 8 feet
 **** Along Dora Kelley Park the detail of the public frontage shall be continuous and as shown on pg. 7.14.
 R.O.W.'s may vary based on existing conditions.
 Sidewalks to be constructed of concrete
 Sections may be finalized during the DSUP process.



f) Garden District

Diagram 9.f.1: Garden District Illustrative Plan

-  BSAP Boundary
-  Garden District Boundary
-  Proposed Buildings
-  Fire Station
-  Open Space
-  Parking Structure
-  Streets
-  Proposed Transitway Stop
-  Major Mid-Block Passage



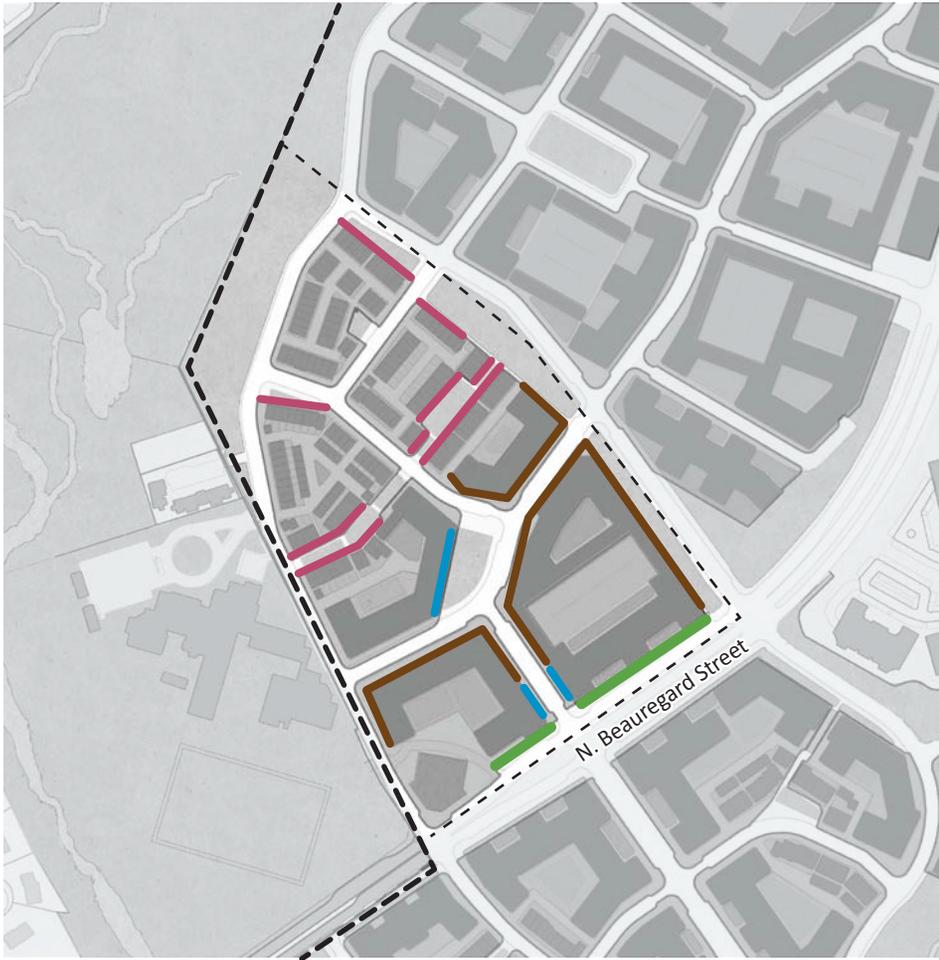


Diagram 9.f.2: Building Setbacks

- Frontage Setback for N. Beauregard St. (10' min. from right-of-way to building)
- Frontage Setback for Optional Retail (16 feet - 25 feet from edge of curb to building)
- Frontages shall have an average setback of 10 feet (from property line to building) for a minimum of 30% of the identified frontage for each building.*
- Frontage Setback frontage along Major Mid-Block Passages and Dedicated Open Space

Notes:

- *Where townhouses are provided, they shall comply with setbacks requirements as set forth in Chapter 4, Section C.
- All other setbacks shall be by building use as described in Chapter 4, Section C.

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ST - 64 - 36

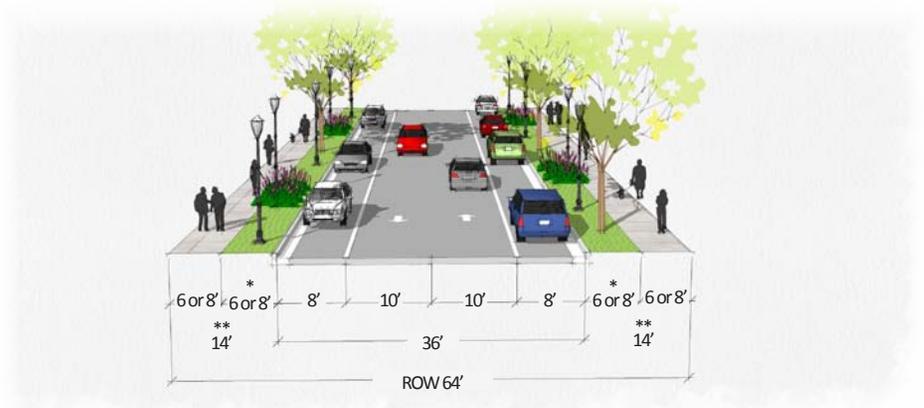
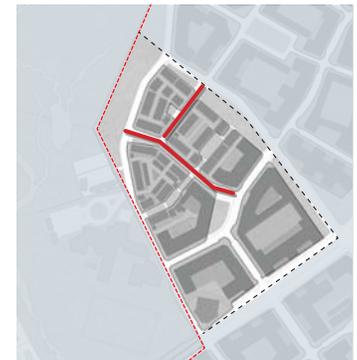


Diagram 9.f.3: Non-Framework Street Sections

— ST - 64 - 36

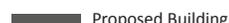
Note:

- * Planting strip at urban locations may be tree wells
 - ** Where 6 feet sidewalk is provided the landscape strip or the tree wells shall be increased in width to 8 feet.
- R.O.W's may vary based on existing conditions.
Sections may be finalized during the DSUP process.



g) Greenway Park

Diagram 9.g.1: Greenway Park Framework

-  BSAP Boundary
-  Greenway Boundary
-  Existing Buildings to Remain
-  Proposed Buildings
-  Open Space
-  Parking Structure
-  Streets
-  Proposed Transitway Stop
-  Required Pedestrian and Vehicular Connection
-  Major Mid-block Passage
-  Pedestrian Bridges: The specific design and location will be determined during the DSUP process.



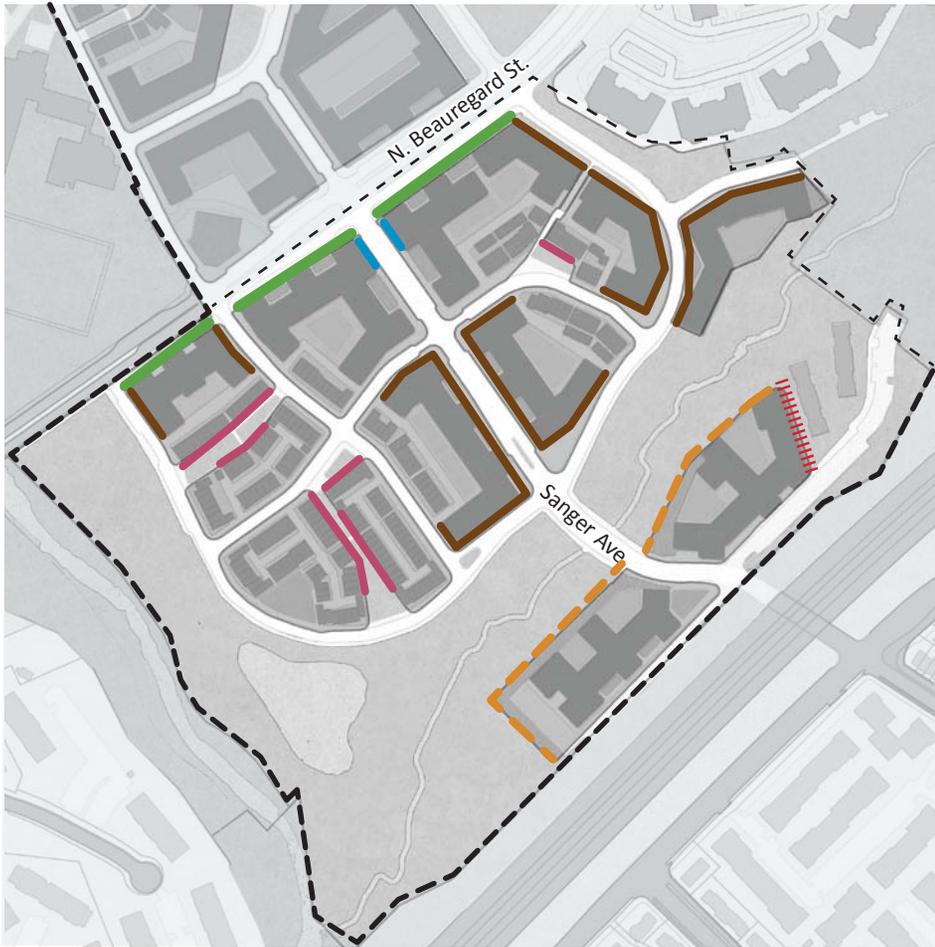


Diagram 9.g.2: Building Setbacks and Transitions

- Frontage Setback for N. Beauregard St. (10' min. from right-of-way to building)
- Frontage Setback for Optional Retail (16 feet - 25 feet from edge of curb to building)
- Frontages shall have an average setback of 10 feet (from property line to building) for a minimum of 30% of the identified frontage for each building.*
- ||||| Frontage Setback frontage along Major Mid-Block Passages and Dedicated Open Space=
- - - - Required Building Transition (See Chapter 4, Section D)

Notes: Recommended Courtyard Building

*Where townhouses are provided, they shall comply with setbacks requirements as set forth in Chapter 4, Section C.

All other setbacks shall be by building use as described in Chapter 4, Section C.

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ST - 64 - 36

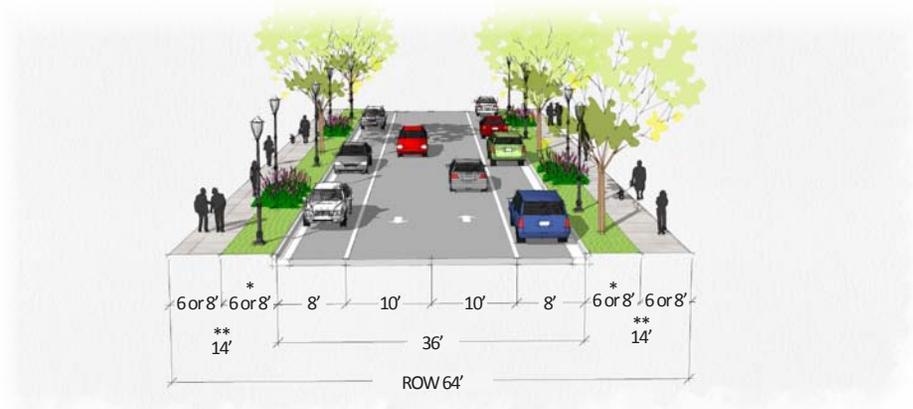


Diagram 9.g.3: Non-Framework Street Sections

— ST - 64 - 36

Note:

R.O.W.'s may vary based on existing conditions.

Sections may be finalized during the DSUP process

* Planting strip at urban locations may be tree wells

** Where 6 feet sidewalk is provided the landscape strip or the tree wells shall be increased in width to 8 feet.

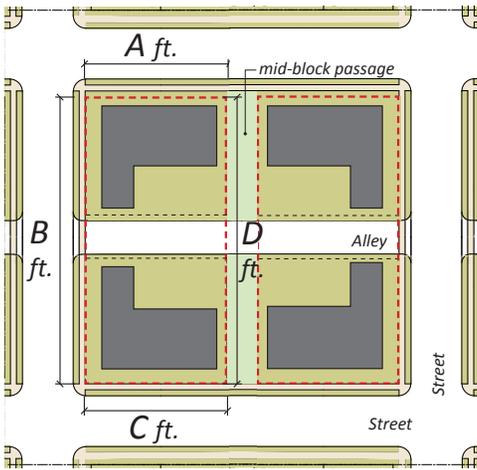


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Chapter 10: Definitions

ILLUSTRATED DEFINITIONS - URBAN STANDARDS & GUIDELINES

BLOCK PERIMETER



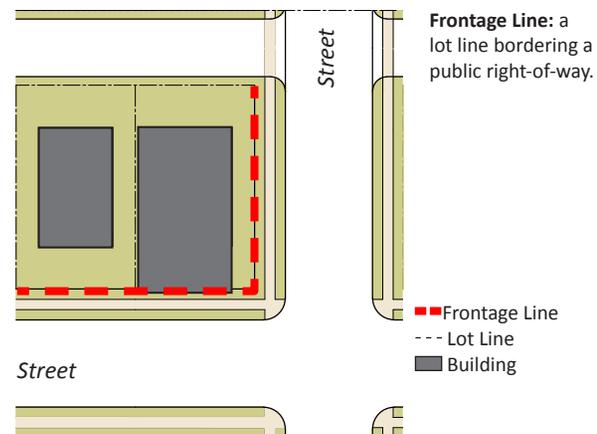
Block perimeter is measured as the right-of-way perimeter adjacent to public streets. If mid-block pedestrian passages are provided, the block perimeter shall be measured from public streets to the mid-block pedestrian passages. As shown in the illustration above.

- - - Block Perimeter
- - - Lot Line
- Building

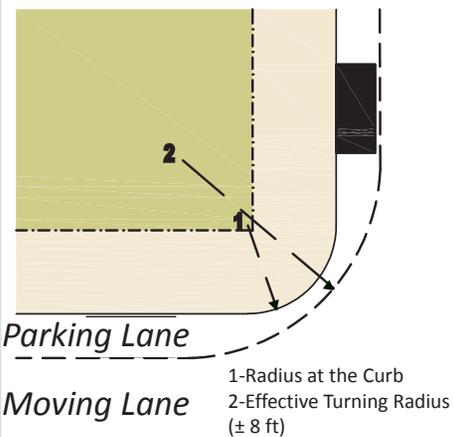
IMAGE EXAMPLE



FRONTAGE LINE AND LOT LINE



TURNING RADIUS



ILLUSTRATED DEFINITIONS - BUILDING HEIGHT TRANSITIONS

BUILDING STEPBACK

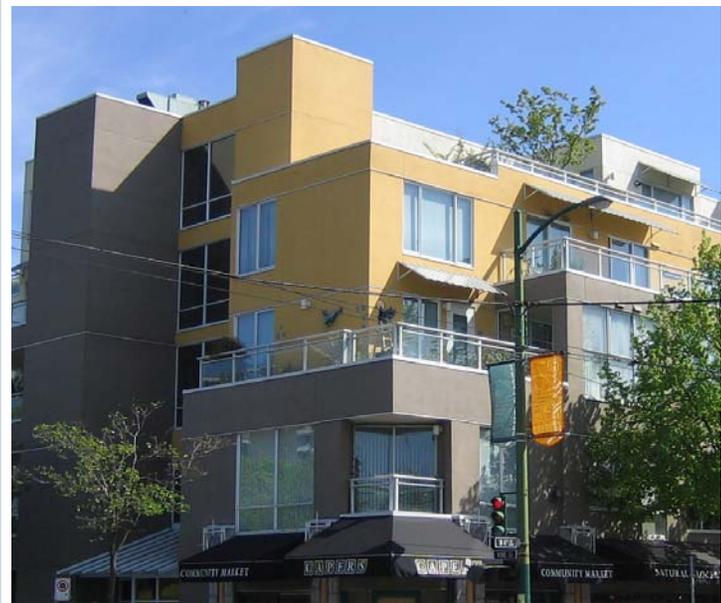


Image Example

BUILDING SHOULDER



Image Example



10.2 DEFINITIONS

ILLUSTRATED DEFINITIONS - BUILDING HEIGHT TRANSITIONS

LANDSCAPE BUFFER

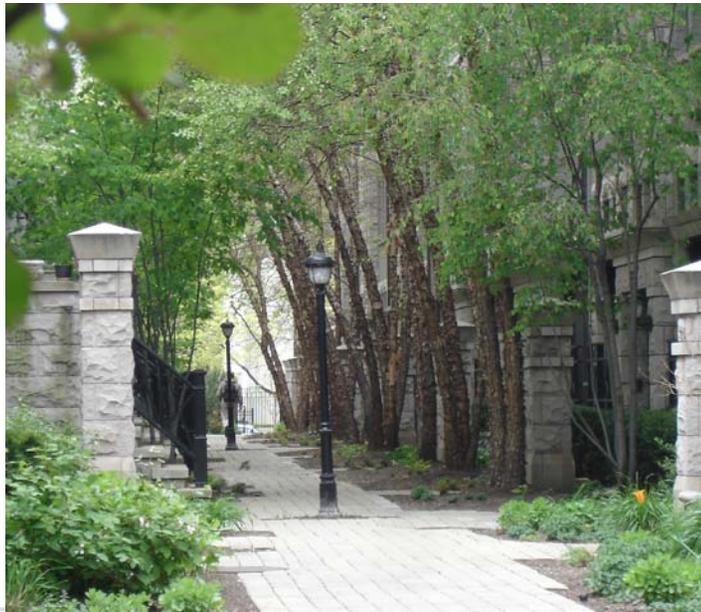


Image Example

COURTYARD



Image Example



Other definitions, not illustrated:

Active uses: Permitted uses designed to encourage human presence within the space and screen above-grade parking garages. Active uses do not include mechanical rooms, storage rooms and loading docks.

Alley: A narrow service access to the rear of buildings and provides service areas, parking access, and utility easements. Syn.: Rear Lane

Trail: A pedestrian way traversing an open space. Trails should connect directly with the sidewalk network at the urban edge. Syn.: Path, Walk.

Liner Building: a shallow building conceived specifically to mask a parking lot or a parking structure.