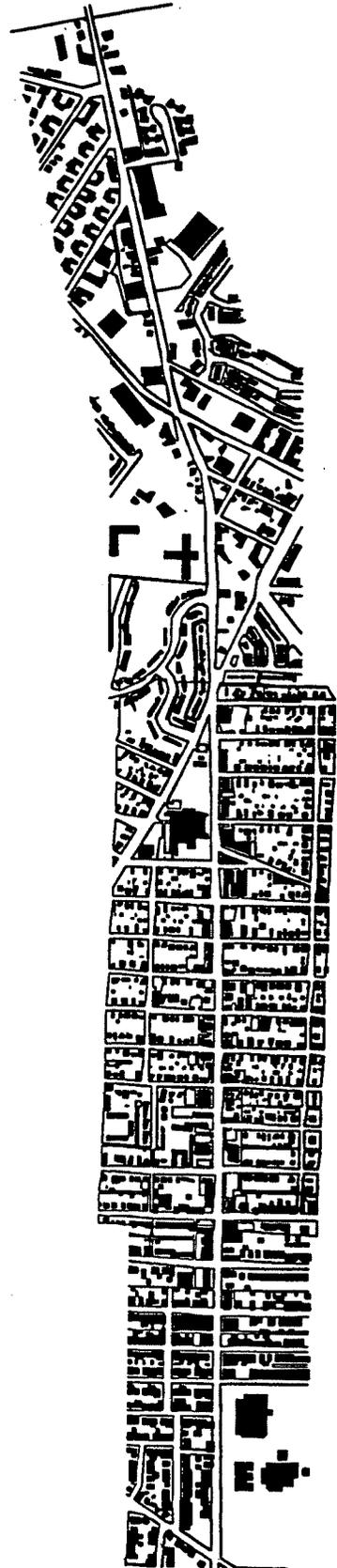


Mount Vernon Avenue Urban Design Guidelines

Department of Planning and Community Development
City of Alexandria, Virginia

Adopted by City Council Resolution
October 16, 1993





MOUNT VERNON AVENUE URBAN DESIGN GUIDELINES

**Department of Planning and
Community Development
City of Alexandria, Virginia**

**Adopted by City Council Resolution
October 16, 1993**

ALEXANDRIA CITY COUNCIL

Mayor Patricia S. Ticer
Vice Mayor William C. Cleveland
Council Member Kerry J. Donley
Council Member T. Michael Jackson
Council Member Redella S. Pepper
Council Member Lonnie C. Rich
Council Member David G. Speck

CITY MANAGER

Vola Lawson

DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

Sheldon Lynn, Director
Nan Laurence, Urban Designer (Principal Author)

MOUNT VERNON AVENUE URBAN DESIGN GUIDELINES COMMITTEE

Rohit Anand, AIA
Elizabeth Sigel Bouchard
Mark Gilliland, AIA
Joan Honeyman, ASLA
Rod Kuckro
Robert Larson, AIA
Marlin G. Lord, AIA
Jonathan Rodvien, AIA
Jim Synder
Kevin Tankersley, ASLA

Special thanks to Robert Larson for the Neighborhood Area maps, Joan Honeyman for the Suggested Plant List, and Peter H. Smith, Principal Staff, Board of Architectural Review, for technical assistance.

MOUNT VERNON AVENUE URBAN DESIGN GUIDELINES

Table of Contents

I. Introduction	
A. Statement of Purpose.....	1
B. Goals.....	1
C. Background.....	2
D. Organization.....	2
E. Use of the Guidelines.....	3
F. Submission Requirements.....	3
G. Development of the Guidelines.....	4
II. Urban Design Guidelines	
A. Building Orientation.....	5
B. Architectural Character.....	6
C. Streetscape.....	9
D. Parking and Access.....	11
E. Fences and Walls.....	13
F. Signs.....	14
III. Appendix	
A. Town of Potomac National Register Historic District.....	15
B. Significant Properties List.....	17
C. Neighborhood Area Maps.....	19
D. Suggested Plant List.....	27
E. Guidelines for Public Right-of-Way.....	29

INTRODUCTION

Statement of Purpose

The purpose of these guidelines is to encourage high quality, thoughtful and appropriate development consistent with the pedestrian-oriented pattern of development along Mount Vernon Avenue. To achieve this purpose, the guidelines provide standards and recommendations for new construction or rehabilitation of properties along the Avenue.

The guidelines focus on urban design issues. Urban design issues include basic design considerations such as neighborhood context, mass, scale and orientation of the building, and the overall physical and visual quality of the area.

•••

Goals

The specific goals of the guidelines are:

- To provide design guidance for development or redevelopment of properties along the Avenue
- To provide review criteria for projects subject to Site Plan approval or Special Use Permit approval by the City
- To encourage an enhanced and visually unified streetscape
- To encourage the compatibility of new construction with the existing pattern of development on the Avenue
- To increase public awareness of neighborhood design issues.

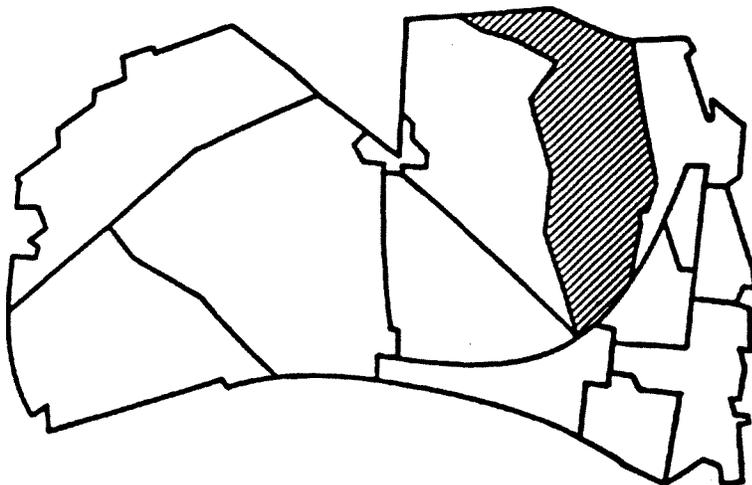


Fig. 2. Potomac West Area in Alexandria

Background

Mount Vernon Avenue is the primary street running through the Potomac West area of Alexandria. It is a neighborhood serving street, linking the neighborhoods of Rosemont, Del Ray, Warwick Village, Lynhaven, Mount Jefferson and Arlandria/Hume Springs.

The Avenue was originally developed in the early part of this century. It was primarily a commercial street, designed to serve pedestrians from the nearby residential areas.

Today, much of the original character of the Avenue is still evident. The buildings face the street, typically maintaining the same setback along the block face. The width and usually the roof line of buildings are very consistent, and the rhythm of spaces between buildings forms a uniform edge to the sidewalk. While there are a number of notable buildings along the Avenue, it is the overall pattern of development which gives the Avenue its distinctive quality and provides a coherent, comfortable pedestrian environment.

A portion of the Avenue has been included in the Town of Potomac National Register Historic District. This portion is included in the area covered by these guidelines (see Appendices A and B); however, the guidelines do not contain specific recommendations relating to historic compatibility or other detailed matters of architectural design.

• • •

Organization

The guidelines are organized by topic, such as Building Orientation, Architectural Character, and so forth. The guidelines also consider four general areas along Mount Vernon Avenue, as well as four "gateways" into the Potomac West area (Fig. 3). The gateways are located on Mount Vernon Avenue at the intersections of Braddock Road, Monroe Avenue, Glebe Road and Four Mile Run; the neighborhood areas are generally located between the intersections shown. (See also Appendix C: Neighborhood Area Maps.) While most of the guidelines apply to the entire length of the Avenue, some guidelines apply only to a specific area or gateway, as noted within each section.

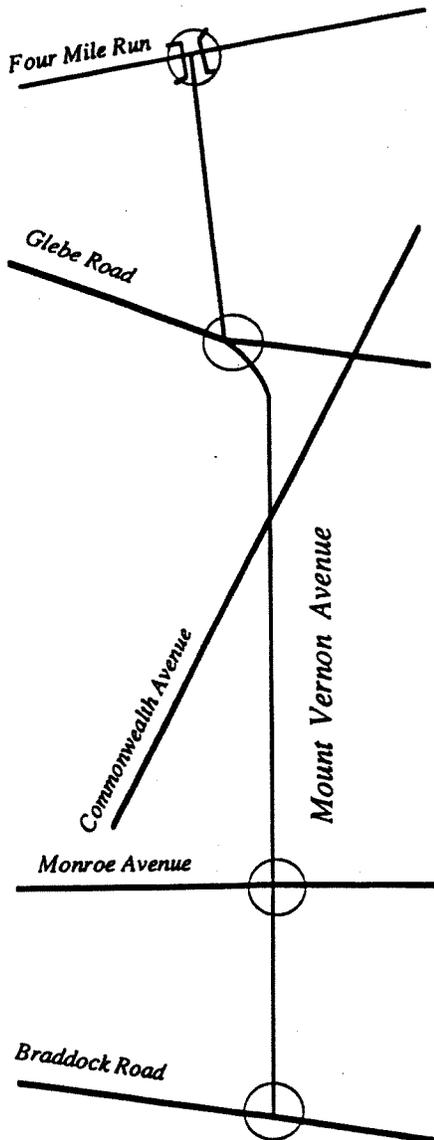


Fig. 3. Mount Vernon Avenue
○ Neighborhood Gateways

Use of the Guidelines

The guidelines are intended to be used by business and property owners, builders and architects of projects along Mount Vernon Avenue, and by citizens, appointed and elected officials and city staff in evaluating these projects.

For projects requiring Site Plan approval or Special Use Permit approval, the guidelines will be incorporated into the review process. The Zoning Ordinance requires the Planning Commission to consider urban design issues such as massing, location and orientation of buildings, parking, pedestrian and site amenities, compatibility with surrounding property and character of the neighborhood. The guidelines are intended to provide assistance to applicants in addressing these issues, and will serve as part of the criteria for public review of these projects.

After the Planning Department staff reviews the application for a project, its recommendation, along with any citizen comments, is forwarded to the Planning Commission for its consideration. For any project requiring a Special Use Permit, the recommendation of the Planning Commission will then be forwarded to City Council for their consideration.

The guidelines are not intended to replace but to support and supplement other applicable codes and ordinances of the City of Alexandria.

•••

Submission Requirements

The following drawings are required as part of all Special Use Permit applications and are requested as part of all Site Plan applications.

- Contextual site plan showing adjacent areas and buildings
- Cross-section through the project showing the height relationship with adjacent buildings and buildings across the street
- Cross-section through the project showing relationships to the sidewalk and street edge (also required for site plans with heights over 50')

Note: Elevations of the proposed structure as well as a massing study, perspective or other three dimensional view are strongly recommended and may be required for Special Use Permit applications by the Director of Planning and Community Development.

Development of the Guidelines

This document addresses an objective of the Master Plan to develop design guidelines for each potential development area, and to incorporate design review into the overall site plan approval process and discretionary zoning review process.

The guidelines were developed by a group of citizens in the Potomac West area working closely with the urban design staff of the Department of Planning and Community Development. The guidelines were reviewed by civic and business organizations in Potomac West, and their comments and suggestions have been incorporated.

The guidelines were recommended for adoption by the Planning Commission, and were subsequently adopted by City Council by resolution as a policy document.

URBAN DESIGN GUIDELINES

BUILDING ORIENTATION

The relationship of a building to its site, the public right-of-way and adjacent buildings is one of the most important components of successful urban design. This section considers the orientation of the building, the setback from the street and site design issues.

- The front of the building must be oriented to and sited parallel to Mount Vernon Avenue, except between Commonwealth Avenue and Glebe Road where topography and other natural features make this orientation difficult. Even in this area, orientation to the Avenue is still recommended wherever possible (Fig. 4).

Buildings located on a corner site should be oriented to and parallel to the side street as well as to Mount Vernon Avenue. The primary orientation should be to Mount Vernon Avenue (Fig. 5).

- The primary building entrance should be on Mount Vernon Avenue.
- The building should maintain the existing pattern of setback along the block. Buildings on corner lots will need to maintain adequate vision clearance, as required by §7-800 of the Zoning Ordinance (Fig. 6).

Where the building cannot maintain the existing setback pattern, landscape elements such as fences, walls or plant materials should be used to reinforce the existing setback line (Fig. 7).

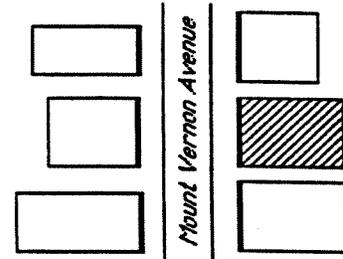


Fig. 4. Front orientation

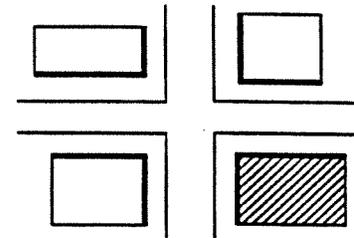


Fig. 5. Corner orientation

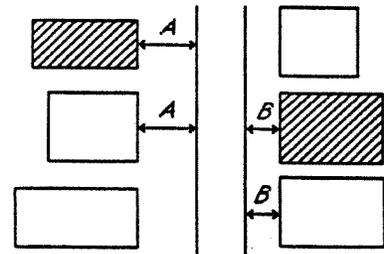


Fig. 6. Maintain existing setback

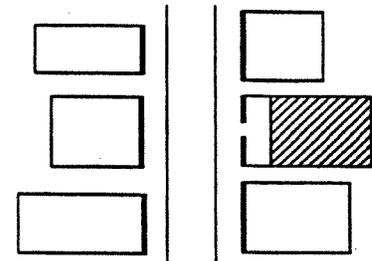


Fig. 7. Reinforce setback line

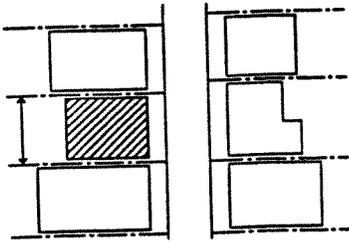


Fig. 8. Occupy maximum lot frontage

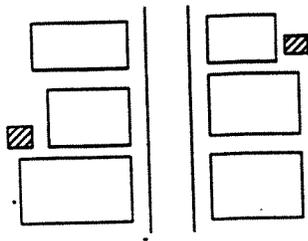


Fig. 9. Locate services appropriately

- The building should occupy as much of the lot frontage along Mount Vernon Avenue as possible. This eliminates irregularly sized gaps between buildings and encourages a discernible rhythm of buildings and open space (Fig. 8).

- The building should be located on the site so that services (such as trash collection) are not visible to pedestrian traffic (Fig. 9).

- The building should be sited so that the privacy, views, sunlight, etc. of any adjacent residential building is preserved wherever possible.

• • •

ARCHITECTURAL CHARACTER

Architectural character is primarily concerned with basic design issues of mass, scale and architectural elements. Mass refers to the overall size and form of a building; scale refers to the perception of the size of a building, building element or space relative to people or other buildings; and architectural elements include basic building components, such as doors, windows, awnings, and so forth. These guidelines are intended to be used for all sides of a building, but particularly those portions visible from the Avenue.

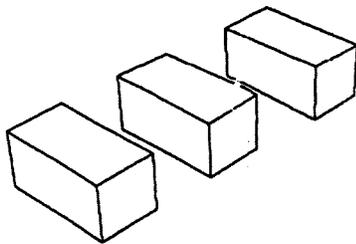


Fig. 10. Maintain compatible mass and scale

- The overall mass and scale of a building, including the roof line and form, should generally be similar to that of other buildings on the block face and the neighborhood in which it is located (Fig. 10).

A public building or a private building at a gateway site may be allowed variation from the "neighborhood norm" for emphasis. These variations might include, for example, a change in height or scale, or architectural elements such as a tower or other landmark feature.

When a new structure is to be located adjacent to a building of historic or architectural significance, particular care should be taken so that the mass of the new structure does not overwhelm the significant structure. (See Appendix B for a list of such structures.)

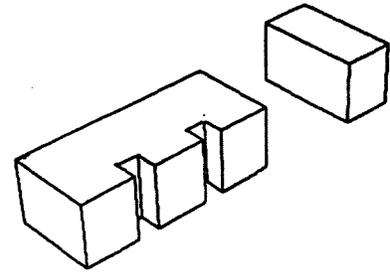


Fig. 11. Maintain typical bay size

- Where the size of available lots would permit construction of larger buildings, the issue of mass and scale is critical. A combination of smaller volumes is recommended, including a typical bay size similar to nearby neighborhood buildings (Fig. 11).

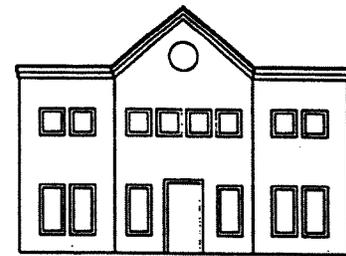


Fig. 12. Provide facade interest

- Architectural elements, patterns of window openings or articulation of bays should all be used to maintain a sense of scale and add interest to a building. Large, blank facades should be avoided (Fig. 12).

- In general, the placement and orientation of architectural elements, particularly windows, should maintain the typical horizontal or vertical emphasis of other structures along the block face (Fig. 13 and 14), with these exceptions:

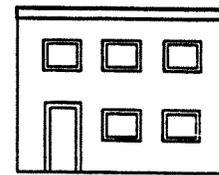


Fig. 13. Horizontal emphasis

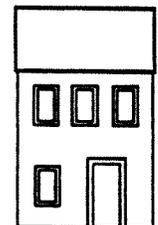


Fig. 14. Vertical emphasis

In general, individual "punched" or framed windows are recommended rather than strip or "ribbon" windows.

Storefront openings, typically including large display windows, are highly recommended for retail uses at street level (Fig. 15).

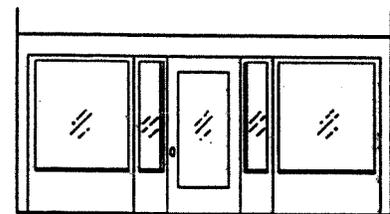


Fig. 15. Storefront windows are recommended

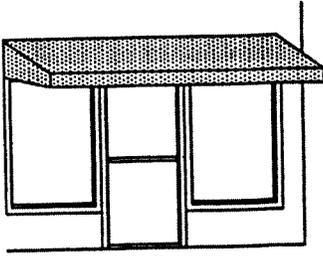


Fig. 16. First floor awnings are encouraged

- Awnings are encouraged for first floor retail uses to provide architectural interest and to encourage pedestrian activity. Awnings over a public way require an encroachment ordinance approved by City Council. Translucent and/or internally lit awnings are not recommended (Fig. 16).

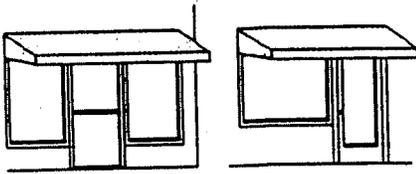


Fig. 17. Coordinate awning design

- Where awnings are used, they should be designed to coordinate with the design of the building and any other awnings along the same block face (Fig. 17).

- The entry to commercial and institutional buildings should be directly from street level.

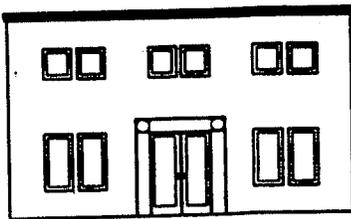


Fig. 18. Prominent entries are encouraged

- The entry to the building should be clearly visible from the street. Prominent entries are encouraged for architectural interest and as an element of scale and orientation (Fig. 18).

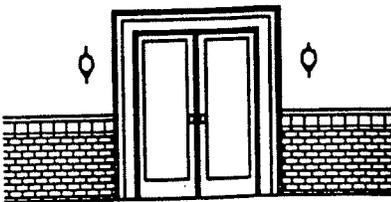


Fig. 19. Include pedestrian-scaled elements

- Architectural components and details, such as a change in materials, ornamentation, and the use of smaller, more human scaled elements are particularly encouraged at the pedestrian level (Fig. 19).

- Attention should also be given to the top of a building. Providing architectural elements such as dormers or details at the cornice level can help to break down the apparent mass of the building, compensate for changes in height and scale between existing and proposed structures, and create interest at the skyline (Fig. 20).
- Buildings should be of those colors and materials which will allow the structure to blend in with the overall fabric of the neighborhood.

• • •

STREETSCAPE

The concept of streetscape generally refers to the quality and character of the public streets and sidewalks and adjacent private property. This section includes guidelines for private property only; Appendix E contains guidelines for the public right-of-way. (See also Appendix D: Suggested Plant List.)

Paving Materials And Borders

- Paved areas between the right-of way and the building facade or entrance area should use the same materials as the right-of-way. (See Appendix E for a list of appropriate materials by area.)
- Accent pavers can be used to emphasize the entrance or other significant areas in the streetscape (Fig. 21). Materials should be compatible with the type and color of the surrounding material.

- An edge treatment is recommended where sidewalk meets grass or planting areas (Fig. 22); an edge treatment is not necessary at building facades. The edge treatment may be:

Precast white concrete tree planter curb (same as existing ones)

Unidecor brick edge for unidecor pavers (by Balcon Manufacturing, or equal)

Landscape timbers are strongly discouraged.

- Driveway aprons for parking and alley access should be poured-in-place concrete.

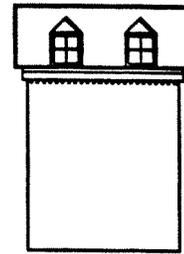


Fig. 20. Provide roof level interest

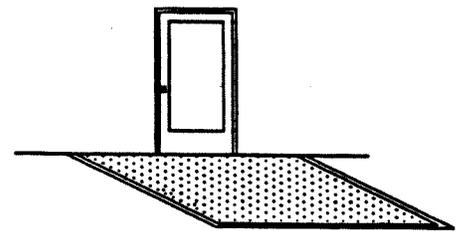


Fig. 21. Accent pavers in sidewalk

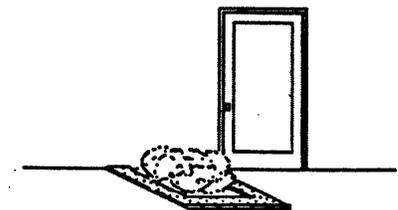


Fig. 22. Edge treatment recommended for planted areas

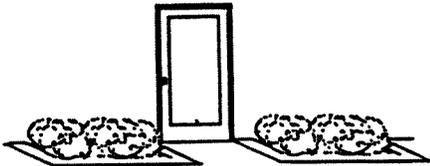


Fig. 23. Planting strips in front of building

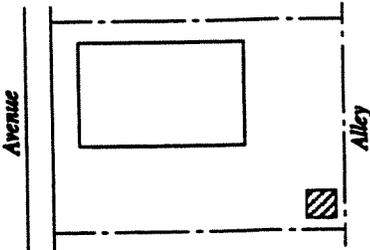


Fig. 24. Transformer located at rear

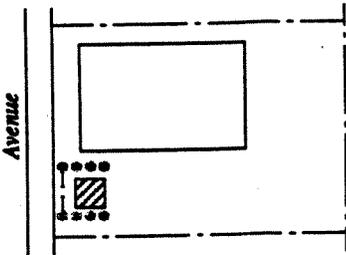


Fig. 25. Transformer located behind front building line

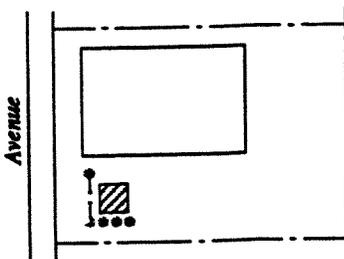


Fig. 26. Transformer screened from street

Bicycle Racks

- Bicycle racks should be provided to encourage bicycle use and to discourage the chaining of bicycles to signs poles, streetlights and trees. They should be placed at convenient, visible, safe, well-lit areas adjacent to building fronts, and located so that the bicycles and racks do not obstruct pedestrian movement. Bicycle racks must be maintained by the property owner.

Planted Areas

- Planting strips on private property in front of buildings are recommended for the area between Four Mile Run and Commonwealth Avenue for multifamily and commercial uses, and between Commonwealth Avenue and Monroe Avenues except for retail uses with storefronts (Fig. 23). The minimum width should be 2 feet beyond roof overhangs.

- Ornamental deciduous shrubs, such as Meidiland Roses, are recommended at gateway corners.

- All planted areas must be maintained by the property owner.

Utilities

- New construction should provide space for pad mounted transformers within the building footprint. If this is not possible, pad mounted transformers should be located adjacent to an alley or at the rear of the property (Fig. 24).

Where that is not feasible, transformers should be located behind the front building line (*i.e.* not between the building and the right-of-way) and should be screened from the right-of-way by evergreen plantings or an opaque enclosure, with an opaque gate (Fig. 25).

Where that is not feasible, and on vacant lots, transformers should be screened from the right-of way by evergreen plantings or an opaque enclosure, with an opaque gate (Fig. 26).

PARKING AND ACCESS

This section includes parking and related areas such as screening and access. These guidelines are intended to minimize visual and physical disruption of the streetscape while promoting efficiency. (See also Appendix D: Suggested Plant List.)

Parking

- In general, parking at the rear of the site is preferred (Fig. 27). Parking between the building and the street is discouraged.
- As an alternative, unless the site is located at a gateway corner, limited parking may be considered along the side of the site with additional parking in the rear (Fig. 28).

Screening

- Appropriate screening of parking areas is necessary. For parking for multifamily or nonresidential development, a landscape buffer zone consisting of a fence, wall or plant materials should be constructed at all property lines adjacent to residential uses. A height of 36" to 42" is recommended. (See also Fences And Walls, Page 12.)
- For parking on the front portion of the site (or along the side at a corner), screening is required to reduce the visual impact of the parked cars and to maintain the street edge for pedestrians. At a minimum, a 6' wide planting strip should be maintained between the right-of-way and the parking area for a landscape buffer zone. For existing parking, the landscape buffer zone may be reduced to a minimum of 3'.

The landscape buffer zone should include a fence, wall and/or plant materials 36" to 42" high. This buffer does not need to be totally opaque; partial visibility into the parking area is recommended for crime deterrence. Flowering trees located at 20' on center are recommended in addition to these materials (Fig. 29).

- All planted areas, fences and walls must be maintained by the property owner.

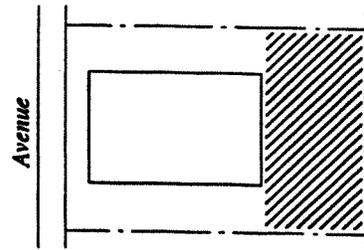


Fig. 27. Parking located at rear

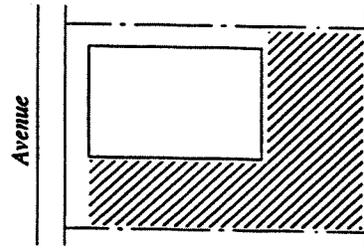


Fig. 28. Limited parking along side

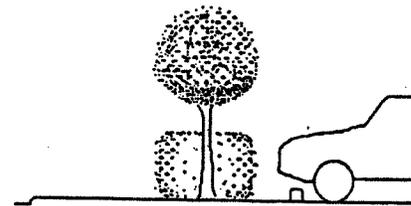


Fig. 29. Landscape buffer zone

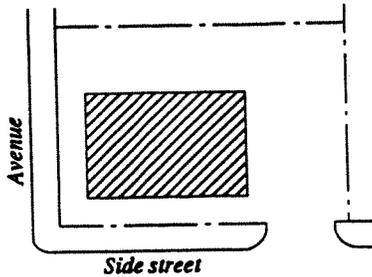


Fig. 30. Parking access off side street

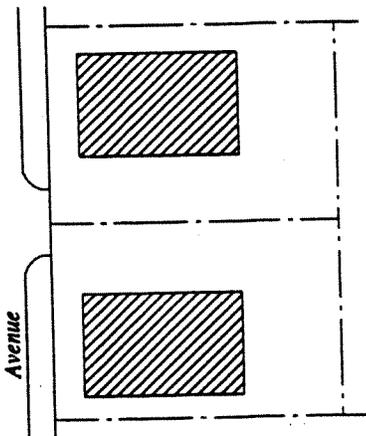


Fig. 31. Shared parking access

Access

- Additional curb cuts along Mount Vernon Avenue are discouraged. Where they are required, a maximum of 22' at the property line is recommended for commercial, institutional or multi-family buildings.
- Development sites with frontage on both Mount Vernon Avenue and a side street should provide parking access off the side street (Fig. 30).
- Adjacent property owners are encouraged to share parking access drives (Fig. 31).
- Trees are recommended in all parking areas whether new or rehabilitated in addition to crown coverage requirements (see §11-4100). For new parking lots, a planting space at least 6' wide by 10' long should be included for shrubs or trees approximately every 12 parking spaces.

Accessibility for Persons with Disabilities

- Accessibility for persons with disabilities must be integrated into the design of the structure. For example, where parking and a public entrance is provided at the rear of a structure, both the front and rear entrances should be designed to be accessible.
- Where accessibility for persons with disabilities needs to be retrofitted, care should be taken to maintain the architectural integrity of the structure wherever possible while still providing appropriate access.

For additional information, refer to the *Accessibility Guidelines for Buildings and Facilities* issued by the U.S. Architectural and Transportation Barriers Compliance Board (56 FR 35455) and the Virginia Uniform Statewide Building Code.

Lighting

- Lighting must be designed and located to meet City standards and as much as possible not to be disruptive to any adjacent residential properties or the public right-of-way.

Service Areas

- Access to buildings for services such as delivery or trash removal should be from the rear of the site wherever possible.
- Trash dumpsters should be screened from view by plant materials, fences or walls. Special consideration should be given to the long term durability of such screening.

FENCES AND WALLS

Fences and walls have many applications such as privacy, buffering, security or definition of edges, and come in a wide variety of types, sizes and materials. Fences are often used to define the border between the public and private rights of way, and therefore need to be designed and sited sensitively so as to contribute to a cohesive streetscape. All fences must follow the requirements of §7-100 to 202, Accessory Uses and Structures, and §7-800, Vision Clearance. (See also Appendix D: Suggested Plant List.)

- Fences and walls should be compatible with the buildings on the property and/or other elements of the streetscape in terms of materials and color.
- The use of chain link and split rail fences is discouraged.
- Landscaping of any area between the public right-of-way and private fence or wall is strongly encouraged (Fig. 32).
- Attractive fences and walls should be used to screen unattractive elements such as trash dumpsters and storage and mechanical equipment areas.
- Fences and walls (or landscape materials) may be used to minimize the effect of irregular spacing between buildings and to maintain the street wall (Fig. 33).
- A landscape border, such as a hedge or other dense plantings, may be used in place of a wall or fence.
- Fences and walls must be maintained by the property owner.

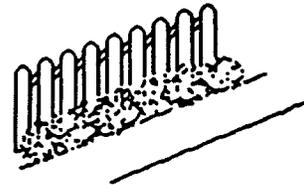


Fig. 32. Landscaping between fence and right-of-way

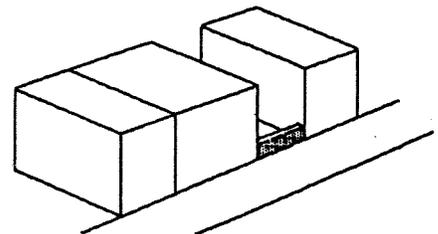


Fig. 33. Fence maintains street wall

SIGNS

Signs play an important part in maintaining the vitality of the commercial uses along Mount Vernon Avenue. They should be designed and sited to identify individual businesses, but must not detract from the image and identity of the entire area. All signs must meet the requirements of §9-100 to 403 of the Zoning Ordinance.

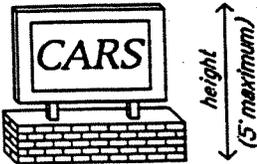


Fig. 34. Ground signs are the preferred sign type

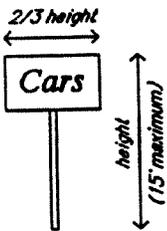


Fig. 35. Maximum sign size

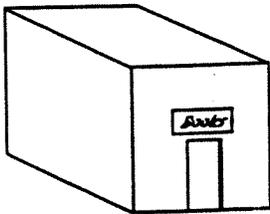


Fig. 36. Orient sign to pedestrians

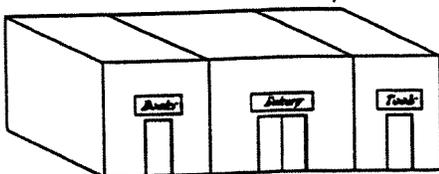
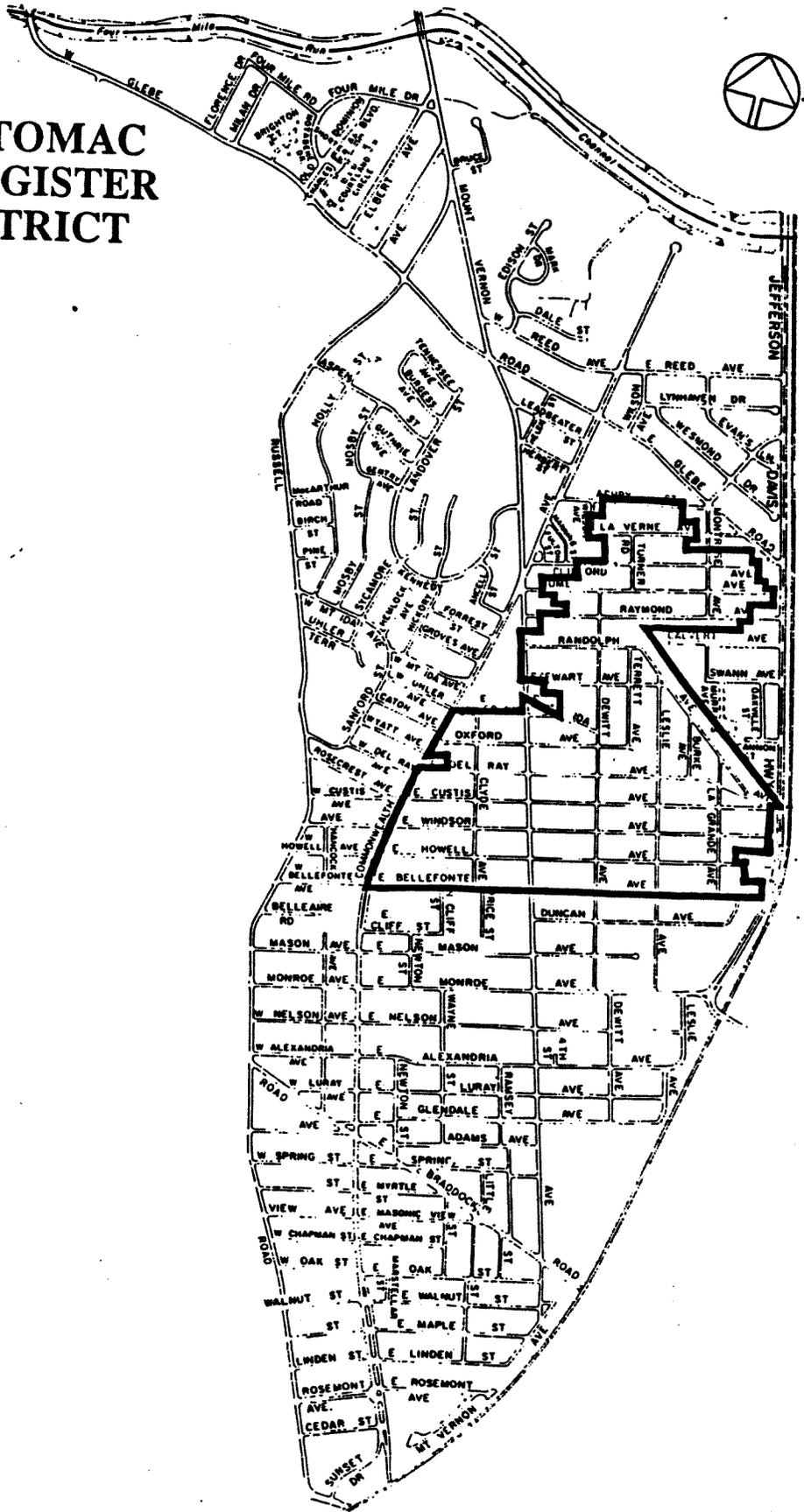


Fig. 37. Coordinate sign design

- The design of commercial signs should be incorporated into the overall design of the structure and site. The color and materials should complement the architecture and the streetscape, not compete with it. The size of the sign should be in proportion with the size of the building.
- Simple, readable designs are strongly encouraged. A maximum of three colors and two type faces is recommended.
- Building signs or ground signs (also known as monument signs) are recommended for the entire length of Mount Vernon Avenue. These types of signs provide excellent visibility for individual businesses and are more compatible with the scale and desired character of the Avenue. The recommended maximum height is 5' (Fig. 34).
- Where pole mounted signs are used, the recommended maximum height is 15'. The recommended maximum width is two-thirds of the height. A single pole is recommended over double poles (Fig. 35).
- The location and size of signs between Braddock Road and Commonwealth Avenue should be oriented to pedestrian traffic (Fig. 36).
- Where a development includes multiple commercial units, the sign for each unit should be of uniform character and location.
- The coordination of sign design along an entire block face is encouraged (Fig. 37).
- Between Braddock Road and Commonwealth Avenue, corporate images such as for fast food establishments should be limited to signs and/or a portion of the facade, such as over the entry, rather than the entire roof or building.

APPENDIX A

TOWN OF POTOMAC
NATIONAL REGISTER
HISTORIC DISTRICT



APPENDIX B

SIGNIFICANT PROPERTIES LIST

The following properties have been found to be contributing structures in the Town of Potomac National Register Historic District:

1903-1905 Mount Vernon Avenue
2000-2004 Mount Vernon Avenue
2005-2009 Mount Vernon Avenue
2006 Mount Vernon Avenue
2008 Mount Vernon Avenue
2018 Mount Vernon Avenue
2100 Mount Vernon Avenue
2103-2105 Mount Vernon Avenue
2104 Mount Vernon Avenue
2109 Mount Vernon Avenue
2201 Mount Vernon Avenue
2205 Mount Vernon Avenue
2206 Mount Vernon Avenue
2213 Mount Vernon Avenue
2301-2302 Mount Vernon Avenue
2309 Mount Vernon Avenue
2400 Mount Vernon Avenue
2401 Mount Vernon Avenue
2402 Mount Vernon Avenue
2403 Mount Vernon Avenue
2406 Mount Vernon Avenue
2423-2527 Mount Vernon Avenue
2601-2609 Mount Vernon Avenue
2701-2795 Mount Vernon Avenue

The following buildings are among the earliest structures along the northern part of the Avenue and are identified with the neighborhood character, scale and development of Arlandria. They are located outside the Town of Potomac National Register Historic District.

3903 Mount Vernon Avenue (Birchmere)
3811-3825 Mount Vernon Avenue (Arlandria Center)
4102 Mount Vernon Avenue (Brill Supermarket)

For additional information on the Town of Potomac National Register Historic District, contact:

Virginia Department of Historic Resources
221 Governor Street
Richmond, Virginia 22319
(804) 786-3143

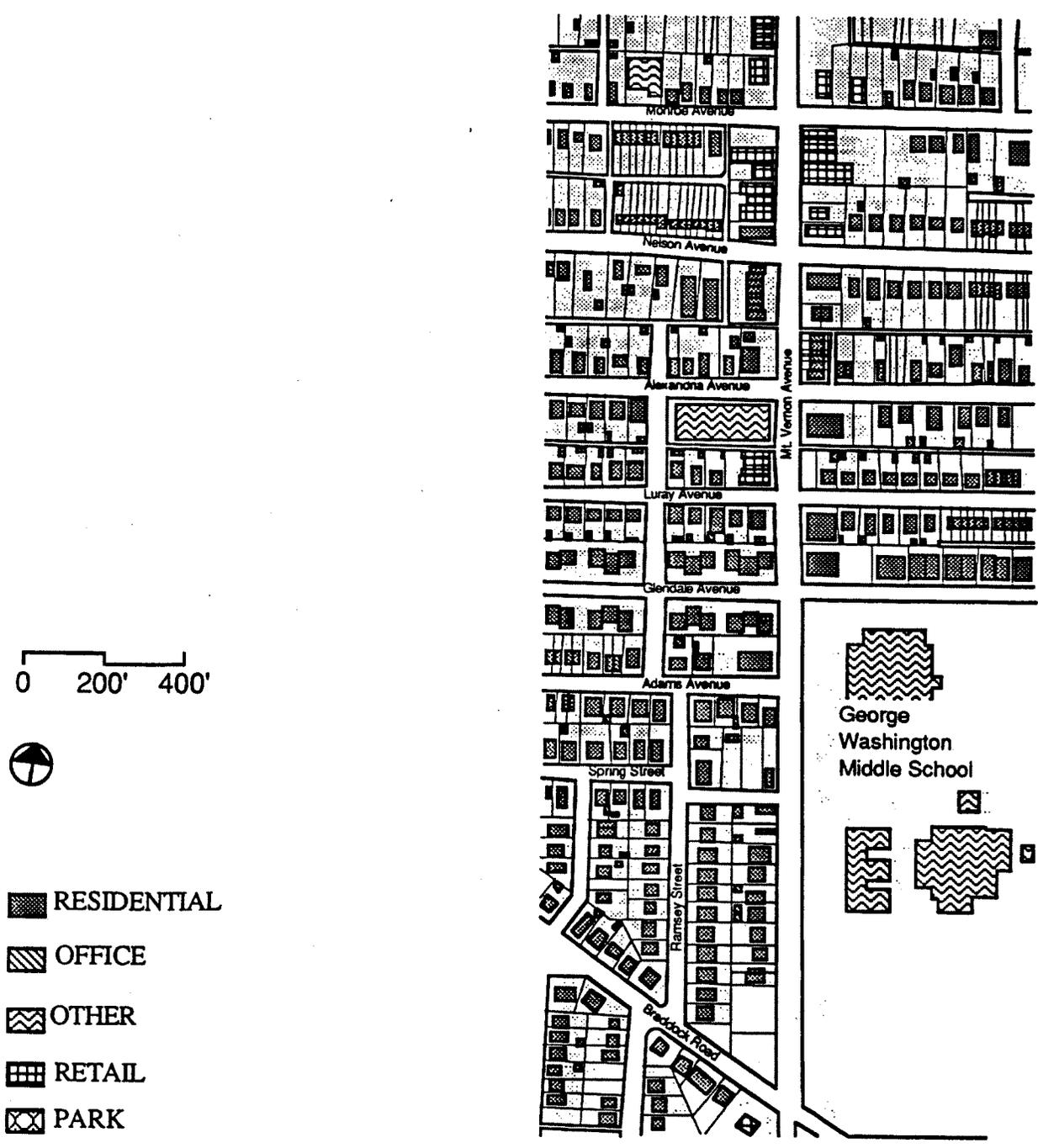
**For additional information on the significance of structures
outside of the District, contact:**

**Joseph Pettiford, Housing Program Coordinator
Office of Housing (Program Implementation)
City of Alexandria
2 West Herbert Street
Alexandria, Virginia 22305
(703) 838-4622**

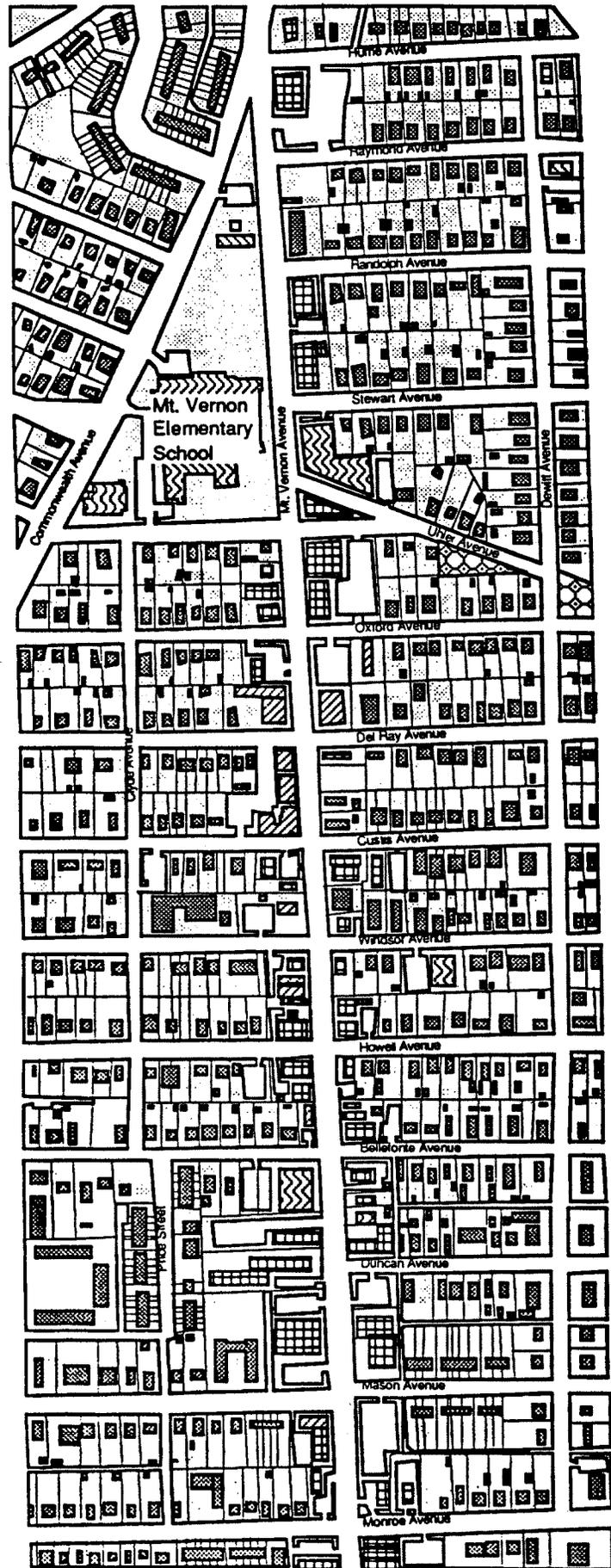
APPENDIX C

NEIGHBORHOOD AREA MAPS

Braddock Road to Monroe Avenue

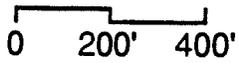
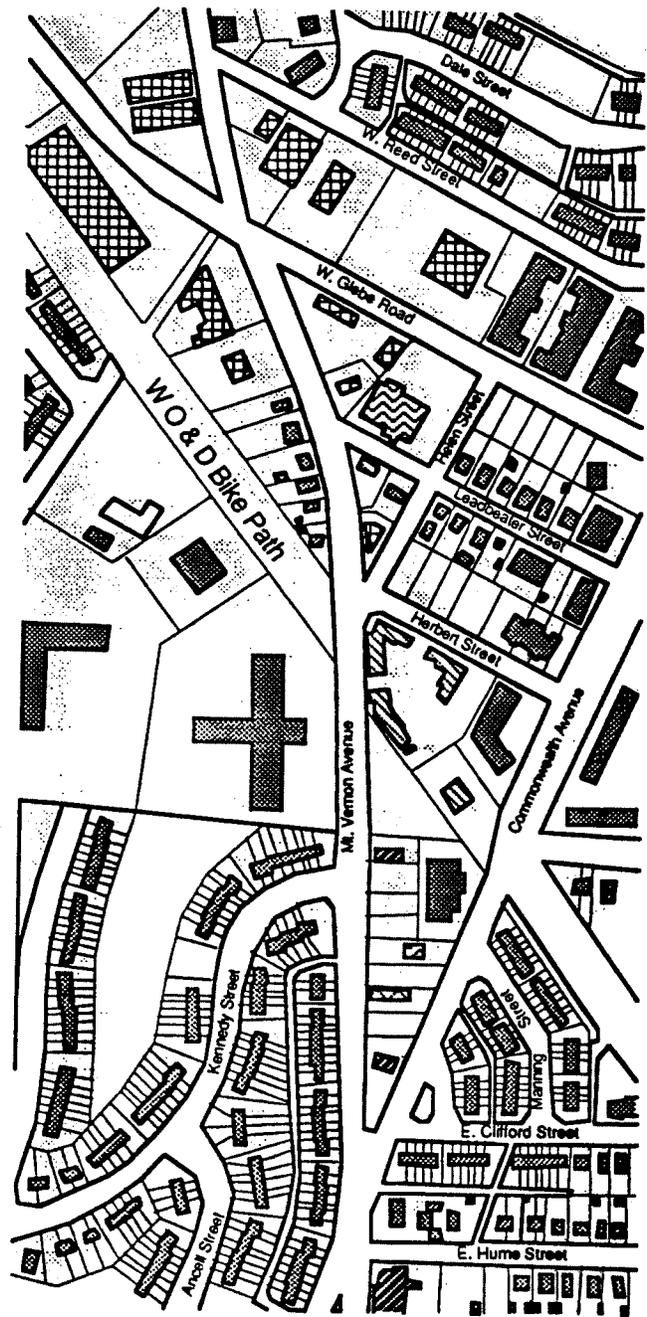


Monroe Avenue to Commonwealth Avenue



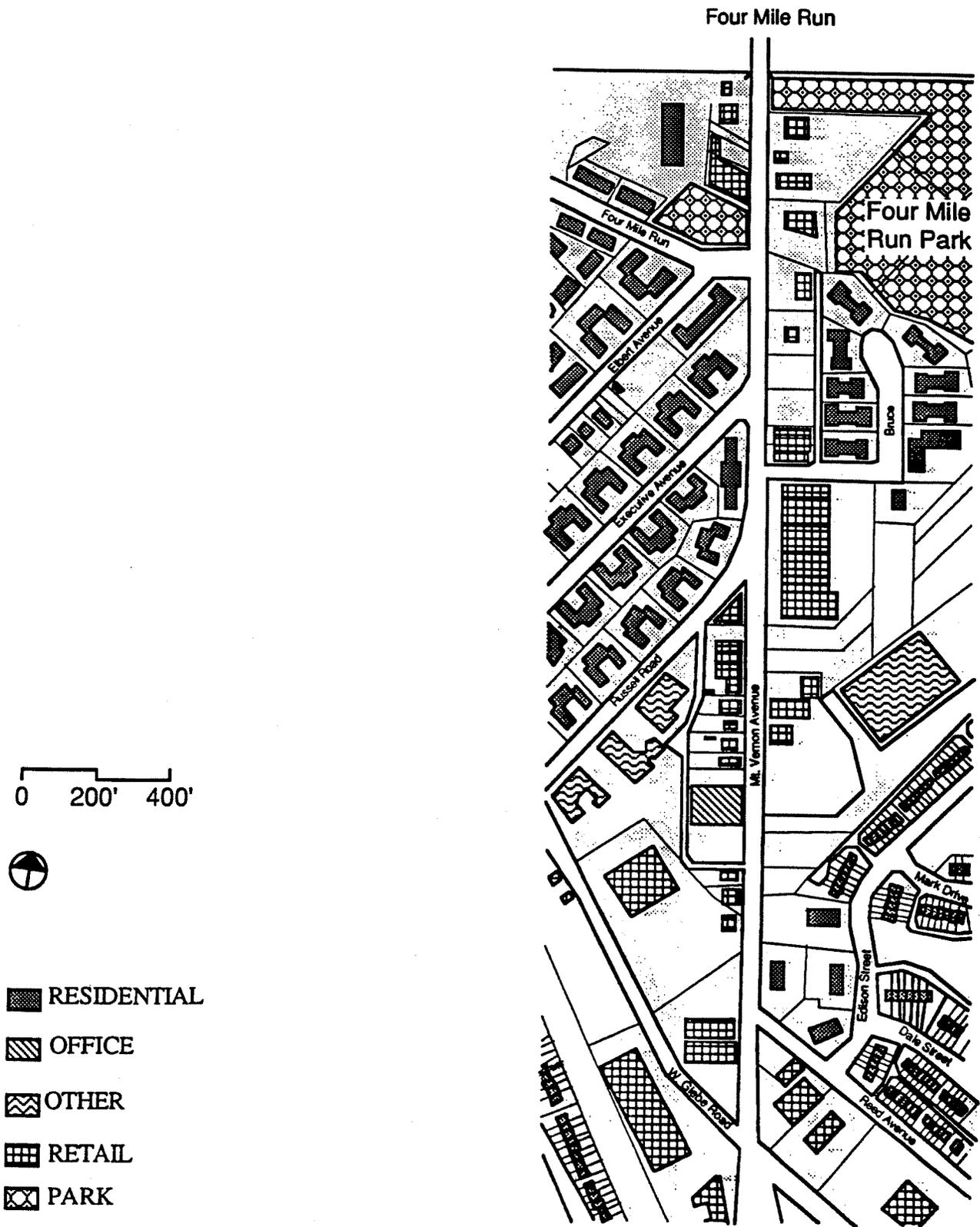
-  RESIDENTIAL
-  OFFICE
-  OTHER
-  RETAIL
-  PARK

Commonwealth Avenue to Glebe Road



-  RESIDENTIAL
-  OFFICE
-  OTHER
-  RETAIL
-  PARK

Glebe Road to Four Mile Run



APPENDIX D

SUGGESTED PLANT LIST

<i>Plant Name and Type</i>	<i>Distinguishing Characteristics</i>	<i>Mature Height and Spread</i>
SMALL DECIDUOUS/ FLOWERING TREES		
Flowering Dogwood (<i>Cornus kousa</i>)	Fall Colors, flowers	30'/25'
Hawthorn (<i>Crataegus phaenopyrum</i>)	Fall colors, flowers, fruit, rapid growth	25'-30'/20'-25' 30'/20'
Crepe Myrtle (<i>Lagerstromia indica</i>)	Flowers, bark	25'/20'
Saucer Magnolia (<i>Magnolia soulangeana</i>)	Flowers, form	10'/20'
Crabapple (<i>Malus</i> sp.)	Flowers, fruit, easy to establish	10'/20'
Yoshino Cherry (<i>Prunus yedoensis</i>)	Flowers, form	35'/25'
LARGE EVERGREEN TREES		
Leyland Cypress (<i>X Cupressocyparis leylandii</i>)	Color, rapid growth, pyramidal	65'/15'
American Holly (<i>Ilex opaca</i>)	Fruit, pyramidal, slow growth	50'/25'
White Pine (<i>Pinus strobus</i>)	Form, rapid growth	80'/50'
Japanese Black Pine (<i>Pinus thunbergiana</i>)	Form, texture	60'/30'
Canadian Hemlock (<i>Tsuga canadensis</i>)	Foliage, texture	60'/30'
MEDIUM/SMALL EVERGREEN TREES		
Foster Holly (<i>Ilex fosterii</i>)	Berries, foliage, pyramidal	30'/20'
Nelly R. Stevens Holly (<i>Ilex x Nellie R. Stevens</i>)	Pyramidal, slow growth	20'/10'
American Arborvitae (<i>Thuja occidentalis</i>)	Foliage, color	50'/15'
DECIDUOUS SHRUBS		
Dwarf Euonymus (<i>Euonymus alatus 'Compacta'</i>)	Fall color, bark	10'/10'
Forsythia (<i>Forsythia x intermedia 'Spectabilis'</i>)	Flower, rapid growth	10'/10'
Spirea (<i>Spirea</i> sp.)	Flower, rapid growth	3'-5'/3'/10'
Korean Spice Viburnum (<i>Viburnum carlesii</i>)	Fragrance, flowers, fall color	6'/3'
Mariesii Viburnum (<i>Viburnum plicatum</i> 'Mariesii')	Flowers, form	8'/12'
Meidiland Rose (<i>Rosa "Meidiland"</i>)	Flowers throughout summer	3'/3'
EVERGREEN SHRUBS		
Evergreen Azalea (<i>Azalea</i> sp.)	Flowers	3'-8'/2'-4'
Japanese Holly (<i>Ilex crenata</i> cultivated species)	Foliage, fruit, slow growth	4'/8'
Juniper (<i>Juniperus</i> sp.)	foliage	1'+/3'+
Cherry Laurel (<i>Prunus laurocerassus</i>)	Flower, foliage	3'-5'/4'-12'
Rhododendron (<i>Rhododendron</i> sp.)	Flowers, evergreen	3'-15'/3'-15'
Yew (<i>Taxus</i> sp.)	Foliage	3'-20'/2'-40'
Manhattan Euonymus (<i>Enonymus kiautschovica</i> 'Manhattans')	Foliage	3'-4'/3'-4'
Red Tipped Photinia (<i>Photinia fraserii</i>)	Foliage	8'-10'/3'-4'

*Plant Name
and Type*

*Distinguishing
Characteristics*

*Mature Height
and Spread*

GROUND COVERS

Lilyturf (<i>Liriope muscari</i>)	Foliage	1'
Bugleweed (<i>Ajuga reptans</i>)	Foliage	1'-2'
English Ivy (<i>Hedera helix</i>)	Foliage	1'
Periwinkle (<i>Vinca minor</i>)	Flowers, moderate growth	1'
Japanese Spurge (<i>Pachysandra terminalis</i>)	Foliage	8"-1'

APPENDIX E

STREETSCAPE GUIDELINES FOR PUBLIC RIGHT-OF-WAY

The guidelines presented here support the effort in recent years to provide a unified aesthetic identity for the streetscape along the Avenue.

Paving Materials And Borders

- Paved areas between sidewalk and building facade or entrance area should use the same materials as the sidewalk.
- Driveway aprons for parking and alley access should be city standard poured-in-place concrete.
- An edge treatment is recommended where sidewalk meets grass or planting areas; an edge treatment is not necessary at building facades. The edge treatment may be:

Precast white concrete tree planter curb (to match existing)

Unidecor brick edge for unidecor pavers (by Balcon Manufacturing or equal)

Landscape timbers are strongly discouraged.

Sidewalk Widths and Materials

- Sidewalk materials should be as follows:

Between Braddock Road and Nelson Avenue: In general, the sidewalk material in this area should be city standard poured in place concrete.

Between Nelson Avenue and Four Mile Run: The sidewalk materials in these areas should be Unidecor concrete pavers in the same color as existing (by Balcon Manufacturing or equal).

- In the following locations, the sidewalk section should include a grass strip from the back of the curb, a 4' city standard concrete sidewalk, then a grass strip to the building edge:

From Braddock Road to Nelson Avenue

From Commonwealth Avenue to Leadbeater Street

- In the following locations the sidewalk should be full width (property line to curb):

Between Nelson Avenue and Commonwealth Avenue

Between Leadbeater Street and Four Mile Run

Street Trees

- Tree selection should match the predominant species on each block.
- Between Braddock Road and Nelson Avenue, trees should be planted in strips rather than tree pits. In all other areas trees should be planted in treepits.
- The standard dimension for a tree pit is 4' x 6'.
- Whenever street trees are removed due to curb cuts, they should be replaced. The replacement trees should be located as directed by the city arborist.
- Wherever possible, trees should be planted 30' on center approximately 30" from the face of curb.
- Ornamental deciduous shrubs, such as Meidiland Roses, are recommended at gateway corners.

Utility Structures

- Pad mounted transformers should not be located within the public right-of-way. If no other suitable space is available, the director of Transportation and Environmental Services may approve pad mounted transformers in the public right-of-way.
- Screening similar to the recommended screening for parking lots will be required. brick sidewalk from back of curb to right-of-way shall be provided.



